

## Statement of Environmental Effects

28 Glebe Street, Parramatta  
Brentwood Residential Aged Care Facility

Submitted to Parramatta City Council  
On behalf of Allity c/o Midson

25 May 2021 | 2200731



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*Group GSA*

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*Ethos Urban*

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## 1.0 Introduction

This Statement of Environmental Effects (SEE) is submitted in support of a Development Application (DA) for a residential aged care facility (RACF) at 28 Glebe Street, Parramatta. The DA seeks approval for:

- Site preparation, demolition works and removal of 4 existing trees;
- Construction of a new three (3) storey plus ground floor RACF with a GFA of 4,869.7m<sup>2</sup> and corresponding FSR of 1.39:1 comprising:
  - A new shared zone and front of house and back of house areas on the Ground Floor;
  - 108 age care beds across Level 1-3 including communal living, dining and lounge areas;
  - Communal open space on the Ground Level as well as terraces on Level 1 to 3;
- New car park comprising approximately 23 car spaces and loading area on the Ground Level;
- Associated landscaping works; and
- 1 x new business identification sign and 1 x temporary freestanding pylon sign during construction (only).

This SEE has been prepared by Ethos Urban on behalf of Allity c/o Midson and is based on the Architectural Plans provided by Group GSA (see **Appendix A**) and other supporting technical information appended to the report (see Table of Contents).

This report describes the site, its environs and the proposed development, and provides an assessment of the environmental impacts and identifies the steps to be taken to protect or lessen the potential impacts on the environment. The application is recommended for approval given the following reasons:

- It is largely consistent with the aims and objectives of the *State Environmental Planning Policy (Housing for Seniors or People with a Disability) 2004* (Seniors SEPP) and other relevant State Environmental Planning Policies as well as the Parramatta Local Environmental Plan 2011 (PLEP 2011) and Parramatta Development Control Plan 2011 (PDCP 2011);
- It will increase the supply of seniors housing commensurate to forecasted demand within the LGA and improve the quality of seniors housing stock available within the LGA. Critically, the proposal intends to provide over 50% of concessional beds to address key affordability issues;
- It is carefully designed to provide a contextual response to the site setting and minimise bulk and scale impacts to adjoining properties;
- It provides a quality architectural design that will contribute to a safe and active, community oriented environment in a contemporary built form;
- Includes supporting technical and social studies which confirm that the environmental impacts associated with the proposal are generally positive and will not give rise to any adverse impacts;
- It will upgrade the existing RACF facility which is outdated and does not meet the needs of the growing population;
- The project will support population growth and demographic changes by allowing the elderly to remain in their community as they age; and
- Is suitable for the site and is in the public interest.

## 2.0 Background

### 2.1 Allity

Allity was founded 8 years ago with the vision to ‘change the face of aged care’ – moving away from hospital style wards with multiple people in each room to a home like environment that people are comfortable placing their family members in.

Brentwood Aged Care has been providing residential aged care to the community of Parramatta since the 1970s. The home historically operated with 100+ beds and provided care to residents in a mixture of multi bed wards. Despite significant investment over the years to provide more common space the home is now operating at 78 beds and is becoming unviable. The existing building isn’t suitable to deliver Allity’s vision and mission:

**Vision:** *From the very beginning, our vision has been clear “To Change the Face of Aged Care” and in doing so, change how aged care is perceived.*

**Mission:** *To “Make every day the best it can be” is how we support the achievement of our Vision. A commitment to making every day the best had to have meaning for everyone, whether it be for our residents, their families or our employees. No matter if you are in the kitchen, driving the bus or working in our offices, being part of Allity means you strive for to do this for yourself and everyone around you.*

Allity’s intention for Brentwood is to redevelop the home and provide a significantly improved accommodation offering of single bedrooms with private ensuites, specifically targeted to residents with low financial means who cannot afford to pay for their accommodation. To this end, Section 5.4 discusses the opportunity for Council to recognise this proposal as Affordable Housing and discharge the responsibility for s7.11 contributions.

### 2.2 Pre-DA lodgement Meeting – Proposed DA

A pre-DA lodgement meeting was held with Parramatta City Council on 3 February 2021 to discuss the proposed development. The key issues raised in the pre-DA lodgement meeting and responses are provided in **Appendix E**.

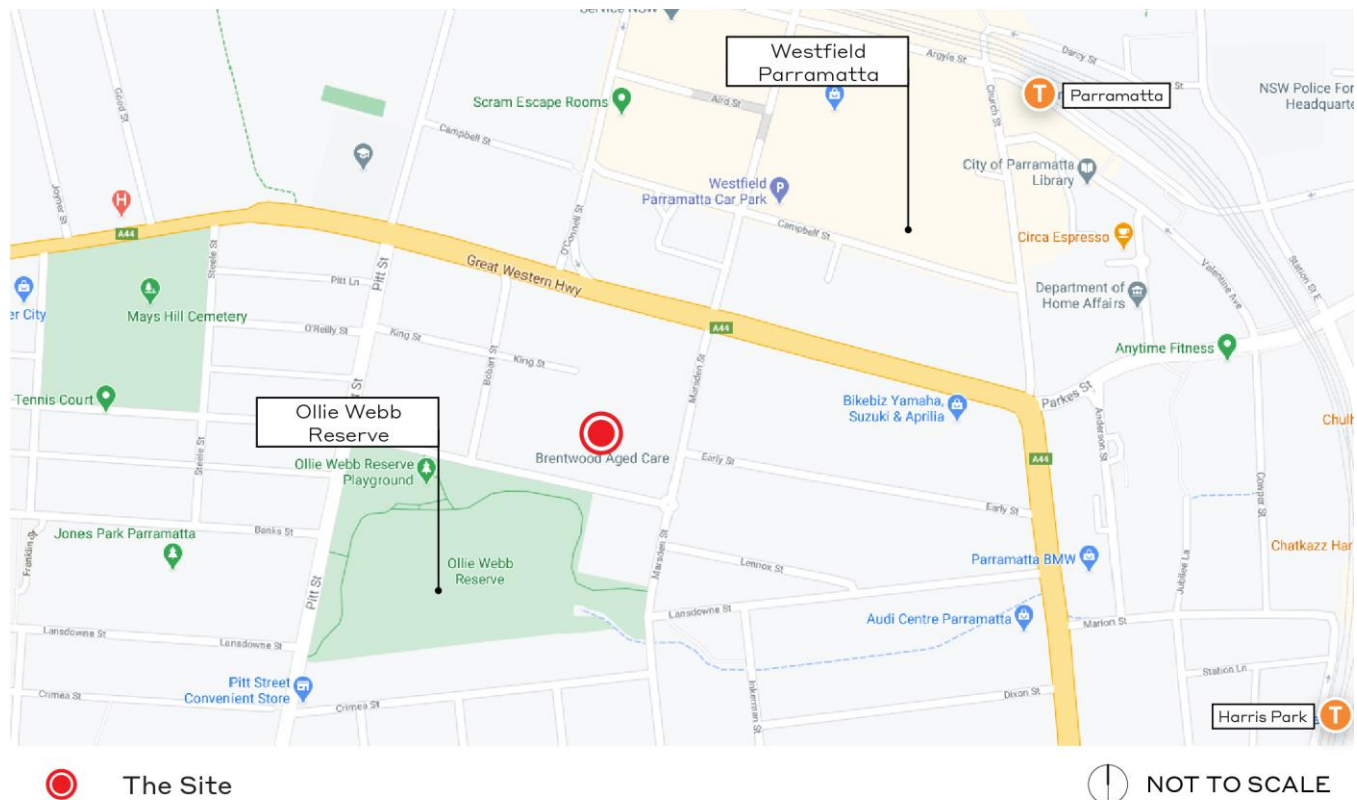
## 3.0 Site Analysis

### 3.1 Site Location and Context

The site is located at 28 Glebe Street, Parramatta and is within the Parramatta local government area (LGA). The site is located just south of the Parramatta CBD approximately 1km away. It is located in a medium-high density residential area, just west of the high density corridor of Parramatta’s ‘Church Street auto alley’.

The site sits directly opposite the Ollie Webb Reserve which provides significant views of a large area of open space and beyond from the site. Along the Great Western Highway to the north of the site, development largely comprises densely developed towers and commercial uses.

The site’s locational context is shown at **Figure 1**.



**Figure 1**      **Locational context**  
Source:      Google Maps / Ethos Urban

### 3.2 Site Description

The site is legally described as Lot 16 in DP554391 and has an approximate area of 3,485.7m<sup>2</sup>. The lot currently comprises the Brentwood Aged Care Facility with vehicular access provided off Glebe Street to an at-grade car park in the front and side setbacks. The site is located on gently sloping land, falling from the Great Western Highway to Glebe Street.

The site has a street frontage of approximately 51m to Glebe Street and is oriented south towards Glebe Street and Ollie Webb Reserve. A survey plan is provided at **Appendix D** and an aerial photo of the site is shown at **Figure 2** below.





**Figure 2 Site aerial**  
Source: Ethos Urban / Nearmap

### 3.3 Existing Development

The site currently comprises the Brentwood Residential Aged Care Facility which provides 78 aged care beds. It also includes various common areas and open space. The existing facility is setback approximately 16m from Glebe Street and adopts an 'H' formation where two households are connected by a central core. The front setback includes an at-grade carpark and a driveway wraps around the eastern and northern setbacks of the site.

The existing development is shown in **Figures 3 to 4**.



**Figure 3 Brentwood Residential Age Care Facility, looking North West towards the site.**  
Source: Ethos Urban



**Figure 4 Pedestrian Entrance into Brentwood Residential Age Care Facility**  
Source: Ethos Urban

### 3.3.1 Access

Vehicular and primary pedestrian access to the existing RACF is provided off Glebe Street. There are two existing driveways on Glebe Street at the eastern and western site boundaries. Off-street parking is provided for 18 cars and a separate loading / servicing area is located at the rear.

### 3.3.2 Topography

The topography falls from the north to the south towards Ollie Webb Reserve and Clay Cliff Creek. The approximate slope is 2 to 3m to the south.

### 3.3.3 Vegetation

Mature street trees are located within the front setback of the site and along the northern boundary. Low lying vegetation and ground covers exist within the area of open space located centrally on the site.

### 3.3.4 Contamination

A Phase 1 Environmental Site Assessment was undertaken by Safe Work and Environments and is included at **Appendix O**. The investigation identifies that the site contains low potential for contaminating land activities to have occurred on the site and a Detailed Site Investigation is not required.

### 3.3.5 Heritage

The site is not listed as a heritage item nor is it located in a heritage conservation area. However, the site is proximate to various locally listed heritage items. These include single storey brick residences located on Marsden Street (item no. 1520). Further discussion is provided at **Section 6.8** and in **Appendix Q**.

### 3.3.6 Bushfire

The site is not located on land that is identified as bushfire prone, nor is located near land that is identified as bushfire prone.

## 3.4 Surrounding Development

The following development surrounds the site:

- **North:** To the immediate north of the site is a series of medium density residential buildings. These are all well in excess of the current height and FSR controls for the site. Further north of the site is Great Western Highway which runs parallel to the Western Motorway (better known as the M4) connecting Parramatta to the Blue Mountains.
- **South:** Directly south of the site is Ollie Webb Reserve which provides a substantial amount of green open space.
- **East:** To the east is a mix of low and medium density residential development. Further east of the building is Marsden Street which provides a connection to the centre of the Parramatta CBD.
- **West:** To the west is medium density residential development. Further west of the building is Bobart Street which connects Glebe Street to the Great Western Highway

The surrounding development is shown in **Figure 5** to **Figure 8** below.





**Figure 5 Surrounding Development – North – 88 Marsden Street**

*Source: Nearmap*



**Figure 6 Surrounding Development – South – Ollie Webb Reserve**

*Source: Ethos Urban*



**Figure 7 Surrounding Development – East – 30 Glebe Street**

*Source: Ethos Urban*



**Figure 8 Surrounding Development – West – 20 Glebe Street**

*Source: Nearmap*

## 4.0 Description of Proposed Development

This application seeks approval for the following development:

- Site preparation, demolition works and removal of 4 existing trees;
- Construction of a new three (3) storey plus ground floor RACF with a GFA of 4,869.7m<sup>2</sup> and corresponding FSR of 1.39:1 comprising:
  - A new shared zone and front of house and back of house areas on the Ground Floor;
  - 108 age care beds across Level 1-3 including communal living, dining and lounge areas;
  - Communal open space on the Ground Level as well as terraces on Level 1 to 3;
- New car park comprising approximately 23 car spaces and loading area on the Ground Level;
- Associated landscaping works; and
- 1 x new business identification sign and 1 x temporary freestanding pylon sign during construction (only).

Architectural drawings illustrating the proposed development are included at **Appendix A**. A photomontage of the proposed development is shown at **Figure 9** below.



**Figure 9** Photomontage of the proposed development  
Source: Group GSA

### 4.1 Numerical Overview

The key numeric development information is summarised in **Table 1**.

**Table 1** Key development information is summarised in

Component	Proposal
Site area	3,485.7m <sup>2</sup>
GFA	4,869.7m <sup>2</sup> (measured in accordance with the Seniors SEPP)
FSR	1.39:1 (measured in accordance with the Seniors SEPP)
Height	12.9m
Beds	108
Car Spaces	23

Landscaped Area	2,116m <sup>2</sup> (60%) (measured in accordance with the Seniors SEPP)
Deep Soil Area	23.53%

## 4.2 Demolition and Site Preparation

The proposed demolition and site preparation involves the removal of the existing building on site, including all internal fittings, structures and existing foundations. In addition, 4 trees will require removal in accordance with the Aboriginal Impact Assessment prepared by Tree IQ and included at **Appendix L**.

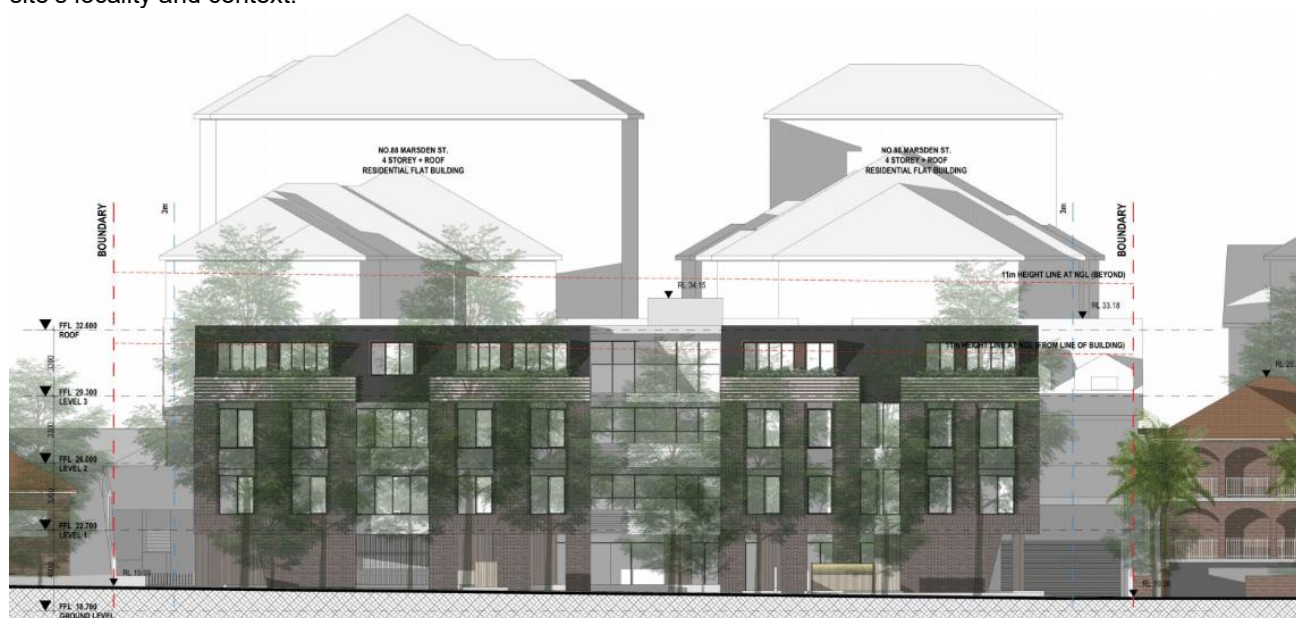
## 4.3 Built Form

The development proposes a new three (3) storey plus ground floor RACF. Above the ground level, which will comprise car parking and back of house / front of house areas, the building will be designed and modulated in an 'H' configuration with two homes connected by a central core area comprising communal lounge and dining spaces as well as terraces. As the proposed facility is located centrally on the site, it adopts a generous setback to the street frontage (7m) as well as to the north (10.3m), east and west (4.5m). This will also allow for the retention of various street trees located on the Glebe Street setback and additional deep soil planting in the side and rear setbacks.

The proposal will include 108 aged care beds across levels 1, 2, and 3, and dining and lounge areas across all floors.

The proposed development will have a maximum GFA (calculated in accordance with the SL SEPP) of 4,869.7m<sup>2</sup> with a corresponding FSR of 1.39:1. The maximum height of the proposed development is 12.9m (calculated in accordance with the SL SEPP).

**Figure 10** below illustrates the proposed built form. The Urban Design Report prepared by Group GSA and included at **Appendix C** also provides analysis of how the proposal responds to adjoining development and the site's locality and context.



**Figure 10** Proposed development as viewed from Glebe Street  
Source: Group GSA



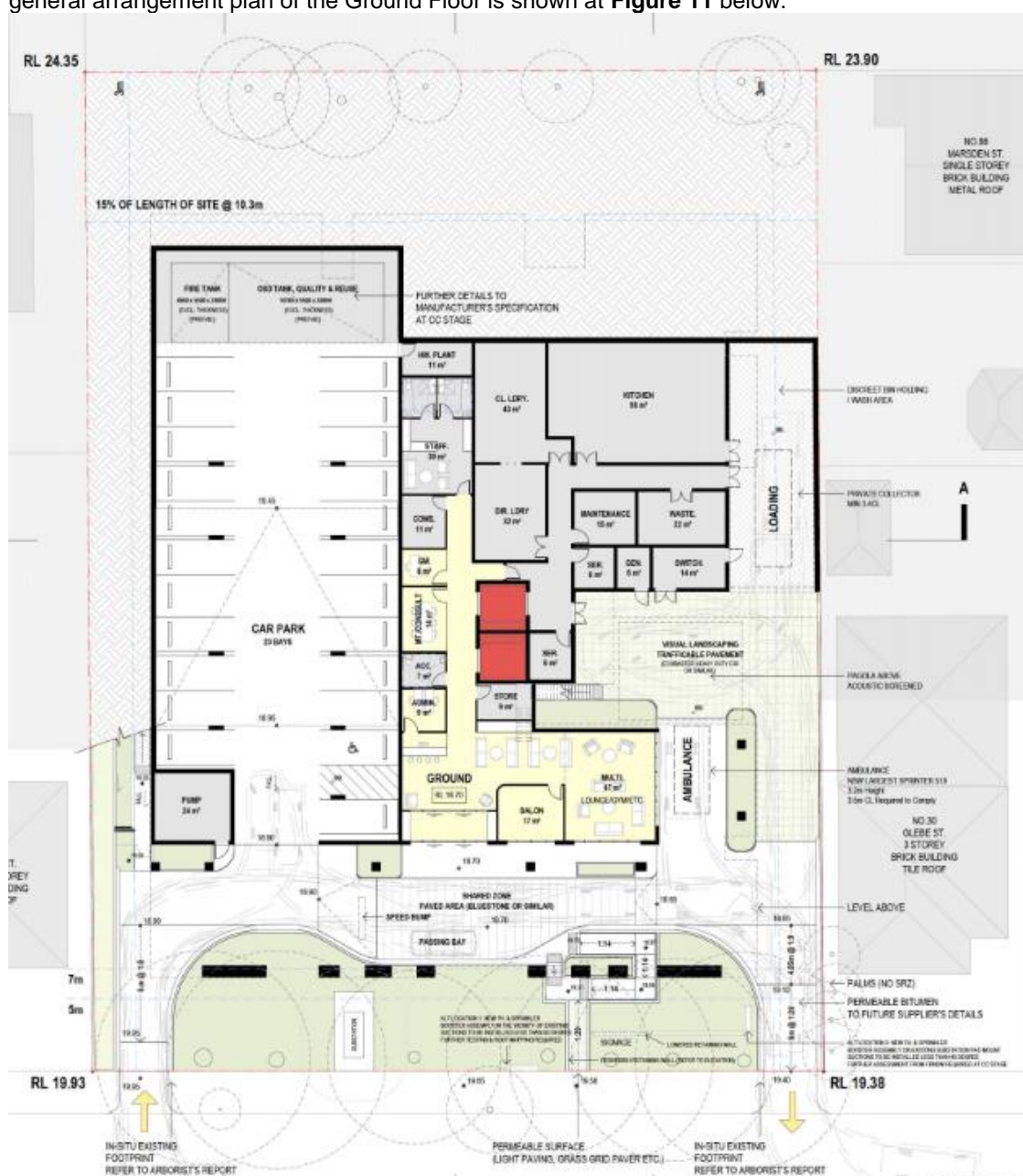
### 4.3.1 Ground Floor

The Ground Level will be the main entry point for residents and visitors to the site. The lobby area will be accessed directly off Glebe Street.

The ground level will comprise of a 23 car parks and a separate ambulance bay on the eastern side of the site. The car park and ambulance bay will be accessed via the existing western driveway and egress from the site will be provided via the eastern driveway. The driveway to the car park will be concealed by the overhang of the building structure above.

The back of house area will comprise waste, storage and laundry areas as well as a kitchen and staff room for use by employees. A discreet bin holding area will be located at the rear.

The front of house area will comprise a lobby area, salon, lounge area and administration space for staff. Permeable paving and visual landscaping will be provided in the eastern boundary area for use by staff. The general arrangement plan of the Ground Floor is shown at **Figure 11** below.



**Figure 11** Ground Floor General Arrangement Plan

Source: Group GSA

### 4.3.2 Levels 1 – 3

Levels 1 to 3 of the proposed development will comprise of 108 beds with a communal dining, lounge and terrace areas.

Each level of the building will accommodate 36 beds separated into 18 beds per 'house'. All rooms will be oriented north-south with the lift core to be located in the centre of each floor. Each level will also include terraces and communal open space for residents.

The general arrangement plan is shown in **Figure 12** below.



**Figure 12** Proposed Level 2 General Arrangement Plan  
Source: Group GSA

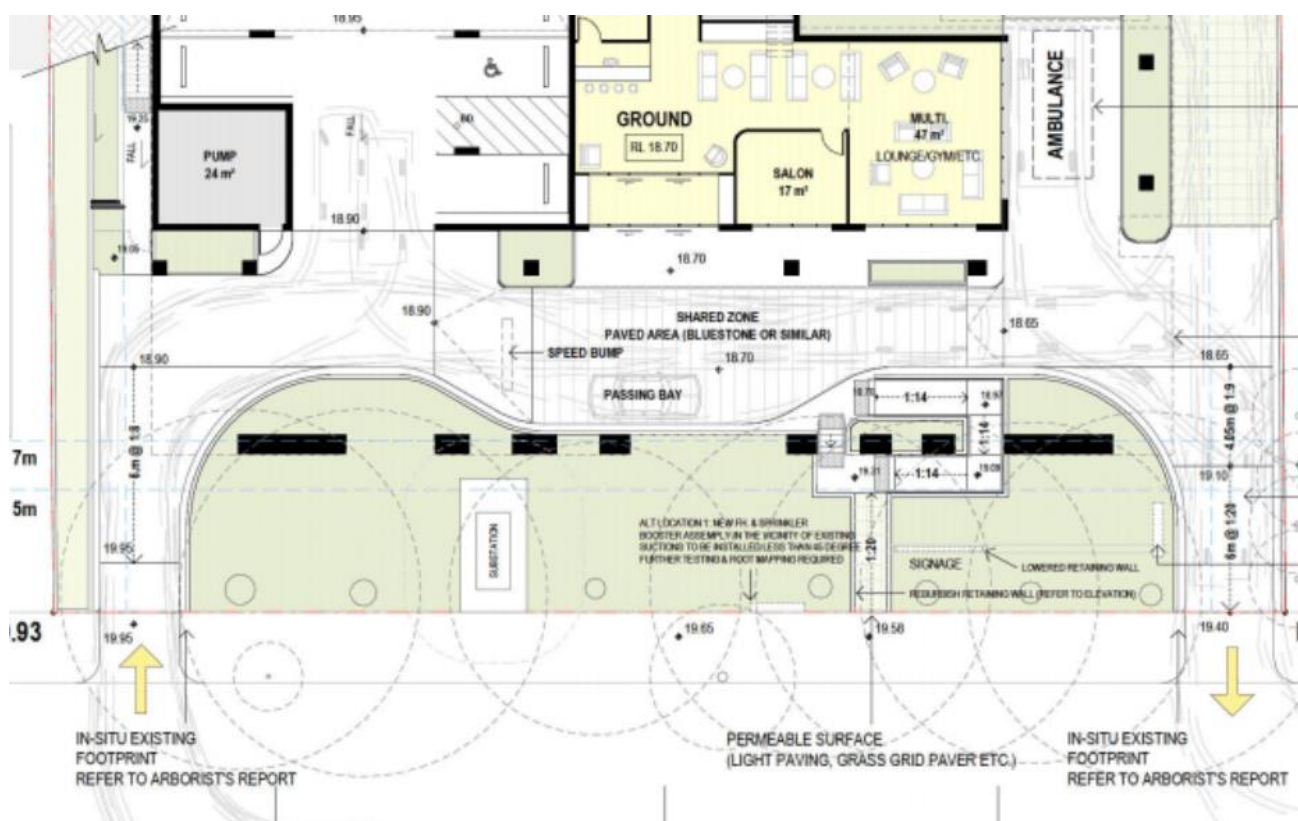
## 4.4 Site Access

### 4.4.1 Vehicular Access and Parking

A car parking area is proposed on the Ground Level and will provide 23 car spaces. Vehicular access to the car parking area, set down / pick up area, ambulance bay and loading / servicing area (for vans and small trucks) will be provided via the two existing separate driveways located on the eastern and western sides of the site's street frontage to Glebe Street. Entry to the site will be provided via the western driveway and exit will be via the eastern driveway.

Waste collection will continue to be undertaken by a private contractor using an 8m long garbage truck.

Detail of the proposed car parking and access arrangements is provided in **Appendix H**.



**Figure 13** Proposed vehicular access from Glebe Street

Source: Group GSA

### 4.4.2 Pedestrian Access

Pedestrian access will be provided via the existing entry point at the midpoint of the site off Glebe Street. The pedestrian access point to the lobby area will be clearly delineated from the driveway through the use of landscaping and signage.

## 4.5 Landscaping

Landscaping drawings have been prepared by Group GSA and are included at **Appendix B**. The overall landscaping strategy has been informed by the site's immediate context, enhancing the outlook from individual units whilst providing an appropriate planting palette to suit existing characteristics of the surrounding area. The key elements of the proposed landscaping include:

- Mass planting and turfed areas with opportunity for productive garden potting (i.e. herbs);



- Terrace areas including large communal spaces and moveable seating / tables as well as central water features;
- Permeable paving on the Ground Floor for staff congregation as well as permeable paving to connect the terrace areas on Level 1.

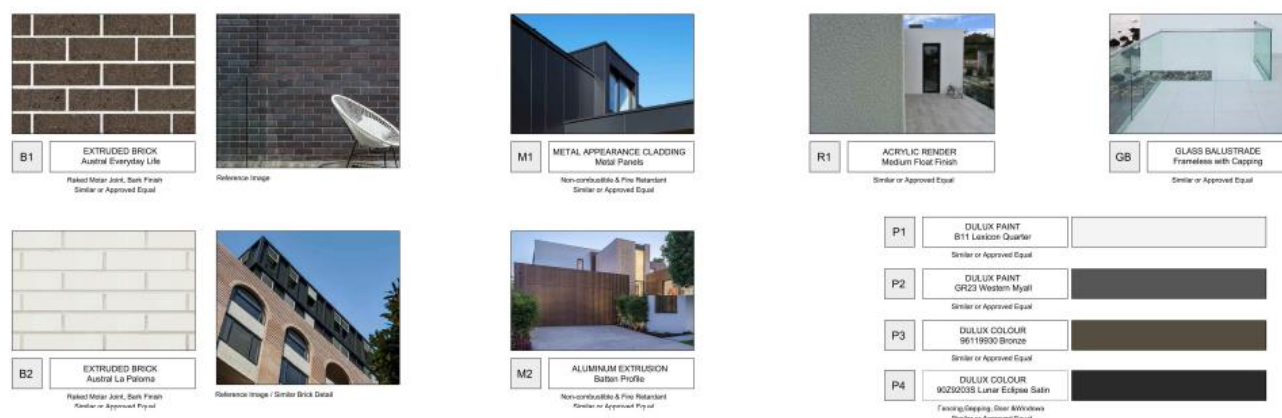
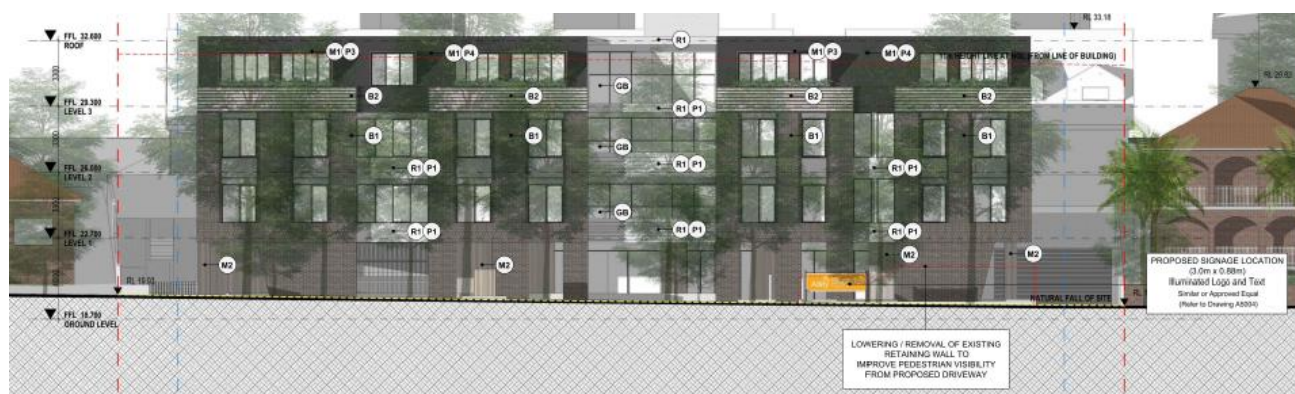
The proposed landscaping and the planting schedule including species size and quantity are included at **Appendix B**.

## 4.6 Façade and Materials

The proposed development will be constructed using various materials and finishes to create a contemporary, high quality development that is compatible with the surrounding built form, while providing visual interest and complementing the character of the adjoining development and streetscape. The proposed material palette provides a range of different fenestration patterns, while breaking down the façade detail into elements that are similar to residential properties in the area. The proposed schedule of materials include:

- Extruded Brick
- Metal Appearance Cladding
- Acrylic Render
- Glass Balustrade
- Aluminium Extrusion
- Outdoor Paint

The proposed materials palette is shown at **Figure 14** below.

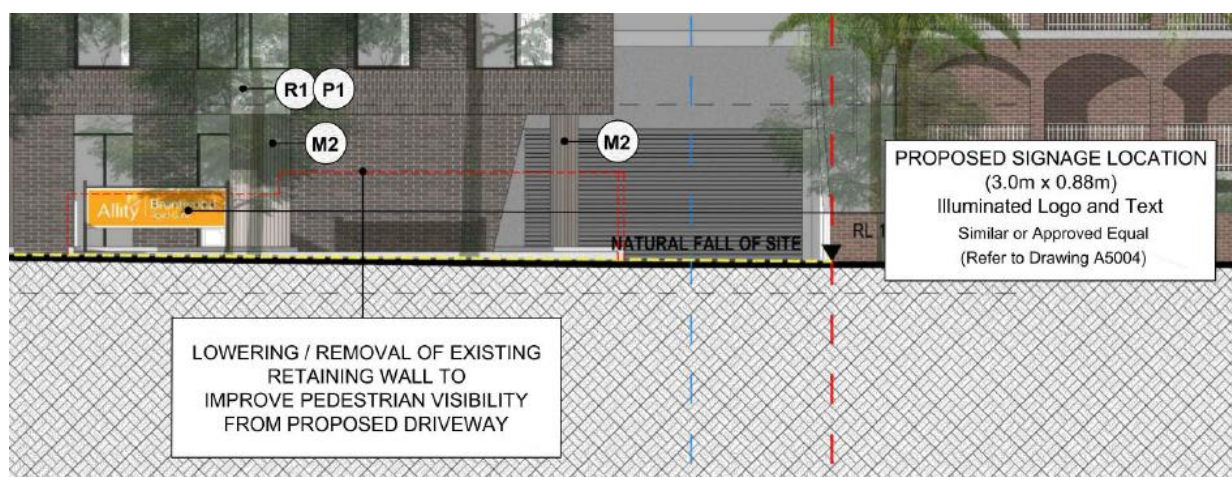


**Figure 14** Schedule of materials  
Source: Group GSA

## 4.7 Signage

The proposed development seeks consent for a new business identification sign fronting Glebe Street to replace the existing sign, as well as a new temporary freestanding pylon sign to be installed during construction works. .

The proposed business identification sign will be 0.88m (H) x 3.0m (W) and will replace the original sign that will be removed as part of the demolition works. The sign will be located approximately 2.44m from the front boundary line and will sit adjacent to the pedestrian path. It will comprise the service operator's name and logo (Allity) and provide clear identification of the service offering on the site, in line with the company's branding. The sign will be affixed to an aluminium extrusion panel as shown in **Figure 15** below.



**Figure 15** Proposed signage zone

Source: Group GSA

The proposed development also seeks consent for a temporary freestanding pylon sign located on the site's Glebe Street frontage during construction works only. The temporary sign will be 2.0mm (H) x 3.6mm (W) and will be fixed to picket post frames. The content of the sign will comprise Allity's logo and branding, with text indicating that the facility is currently being updated and will provide relevant contact details. The proposed freestanding pylon sign will only be installed on a temporary basis whilst construction works are being undertaken on site. An example of the signage is provided in **Figure 16** below.



**Figure 16** Proposed temporary freestanding sign

Source: Group GSA

## 4.8 Waste Management

The proposed development includes a waste holding / management area on the Ground Floor with direct access to the loading dock for waste removal by a private contractor. The private contractor will remove all collected general and recyclable waste. The waste management area will be managed onsite by the Maintenance Caretaker / Manager.

All waste bins within the RACF will be provided in accordance with the relevant Australian Standards. The following bin types and quantities will be provided:

- General waste – 1,100L x 3;
- Co-mingled waste – 1,100L x 2;
- Medical waste – 120L x 2;
- Cytotoxic waste – 120L x 2;
- Secured paper waste – 240L x 1;
- Green / garden waste – 660L x 1; and
- Fluid waste – 1,265mm x 645mm bunded pallet x 1.

Further discussion is provided in **Section 6.7** and at **Appendix N**.

## 4.9 Stormwater Management

The proposal will include new OSD tank at the rear of the undercover car park. The OSD tank will be a minimum of 158m<sup>3</sup> plus 10m<sup>3</sup> for reuse volume.

Further detail is provided in the Civil Plans and Statement prepared by Northrop and included at **Appendix I**.

## 4.10 Services and Utilities

An Infrastructure and Services Statement has been prepared by JHA and is included at **Appendix S**.

### Gas

A Jemena gas metre is located on Glebe Street and is available for connection to the proposed development. JHA confirm there is anticipated to be sufficient capacity to cater for the proposed load required by the development.

### Water and Sewer

There is an existing Sydney Water sewer main available for connection on Glebe Street, at the southern end of the site. JHA confirm there is anticipated to be sufficient capacity to cater for the proposed load expected from the development.

An existing Sydney Water water main is located on Glebe Street and is available for connection to the proposed development. JHA confirm there is sufficient flow capacity required for potable water for the development.

### Power

The site is currently fed from an underground LV connection from an Endeavour Energy substation kiosk within the front setback of the existing facility. The existing substation will be replaced as part of the proposed development due to capacity and age of the existing substation kiosk.

Endeavour Energy has been approach to determine the additional load required to accommodate the proposed development. This has resulted in a new substation being located in the middle of the front boundary to enable a seamless transfer of power, after which the existing substation will be decommissioned. A new site Main Switchboard will be built and minor augmentation will be made internally to provide electricity to the proposed development.

## Telecommunications

The existing RACF is serviced by a single communication network being Telstra. An existing on street communications pit, via an overhead reticulation to the existing site main is provided. Minor internal augmentation will be made to provide telecommunication services to the new development. The connection will follow the existing reticulation and will terminate within the proposed communications room.

### 4.11 Plan of Management

An Operational Plan of Management (OPoM) has been prepared by Allity and is included at **Appendix R**. The OPoM details the management of the facility during operation including staffing requirements, emergency services, site security, food and medical supplies.

### 4.12 Construction Management

A detailed Construction Management Plan (CMP) will be prepared by the appointed contractor prior to the commencement of works in accordance with standard conditions of consent. The CMP will address the following matters:

- Construction hours;
- Material management;
- Construction traffic management;
- Health and safety;
- Equipment / material staging and parking;
- Dust control measures; and
- Methods for disposal and demolition of waste.

It is noted that the proposed development will not be constructed in stages for the simplicity of construction and for the wellbeing of residents. We note however it may be possible that more than one Construction Certificate is issued for the site and therefore we request that the word 'relevant' is inserted before 'Construction Certificate' in relevant conditions.



## 5.0 Environmental Planning Instruments

This section considers the planning issues relevant to the proposed development and provides an assessment of the relevant matters prescribed in section 4.15(1) of the *Environmental Planning and Assessment Act 1979* (EP&A Act).

The DA's consistency and compliance with the relevant environmental planning instruments is considered in the sections below. The key standards and guidelines highlighted in the table are discussed in the following sections of this environmental assessment.

### 5.1 Strategic Plans and Policies

The consistency of the proposed development with the relevant strategic plans and policies is detailed in **Table 2** below.

**Table 2 Summary of consistency with relevant strategic plans**

Plan	Comments
<b>Relevant Strategic Plans</b>	
A Metropolis of Three Cities – The Greater Sydney Region Plan	<p>The Department of Planning, Industry and Environment (previously the Department of Planning and Environment), released the final Greater Sydney Region Plan in March 2018. The plan aims to ensure land use and transport opportunities develop more equitably across Greater Sydney.</p> <p>The proposed development is considered to be consistent with the objectives of the Plan as it will:</p> <ul style="list-style-type: none"> <li>• Provide services and infrastructure to meet communities' changing needs;</li> <li>• Ensure communities are healthy, resilient and socially connected;</li> <li>• Support greater housing supply in the Parramatta LGA; and</li> <li>• Ensure housing is more diverse and affordable.</li> </ul> <p>The proposed development will increase the diversity of housing types for seniors within the Parramatta LGA by providing an improved and upgraded facility to meet modern day seniors housing standard in an accessible location, within proximity to a range of services and infrastructure.</p>
Central City District Plan	<p>The site is located within the Central City District. The Central City District Plan was released in March 2018 and provides a range of priorities and actions to support strategic growth of Sydney's Central City District. The Central City District Plan recognises the large proportion of growth in older people to 2036, with a 95% increase in people aged 65-84 years and a 183% increase in the 85+ year age group. This equates to 16% of the District's population being aged 65 years and over, up from 11% in 2016.</p> <p>The Plan recognises that more diverse housing types and medium density housing will create opportunities for older people to continue living in their community, while being close to their established neighbourhood and in proximity to family, friends and existing infrastructure and services. Accordingly, the proposed development has the ability to align with <i>Planning Priority C5 - Providing housing supply, choice and affordability with access to jobs, services and public transport</i>, as it:</p> <ul style="list-style-type: none"> <li>• Recognises the demand for different housing types and tenures within accessible locations, specifically, aged care facilities.</li> <li>• Responds to housing preferences in Greater Sydney where people generally prefer to remain within their local area, with 82% of residents moving into a new home within 15km of their former residences; and</li> <li>• Fosters a healthy, creative, culturally rich and socially connected community.</li> </ul> <p>Accordingly, the proposed development is consistent with these aims, by demonstrating a commitment to planning for demographic change through the delivery of greater housing choice and community support for seniors. The proposed development will also increase capacity on an existing aged care site and provide an upgraded servicing offering to meet the needs of existing and future residents and to respond to the needs of modern day aged care standards.</p>
Local Strategic Planning Statement City Plan 2036	<p>Parramatta's Local Strategic Planning Statement City Plan 2036 (LSPS) sets out a 20 year land use planning vision. The LSPS came into effect on 31 March 2020.</p> <p>The LSPS estimates that population growth in Parramatta will increase from 235,000 in 2016 to 433,000 in 2036. Amongst other things, the LSPS encourages a mix of housing types including seniors housing. While it is expected that the Parramatta LGA will meet its estimated housing targets, the renewal of the existing aged care facility will not undermine the strategic objectives of the LSPS and as the proposed development is located on an existing site, it will allow for the upgrade of</p>

	services to better meet the needs of the growing population and to allow for existing residents residing in the Parramatta LGA to age in place.
<i>Local Housing Strategy 2020</i>	<p>Parramatta's Local Housing Strategy 2020 provides direction about when and where future housing growth will occur to 2036 and beyond, consistent with the Central City District Plan. The Housing Strategy identifies that the 70-84 and 85+ age groups will see a 64% growth to 2036 and therefore future housing will need to accommodate older and less able residents, such as seniors and elderly residents.</p> <p>In addition, the results of a community survey undertaken by Council that has informed the Strategy, showed that respondents identified the need to create more seniors housing to increase housing diversity in the LGA.</p> <p>The proposed development is capable of directly aligning with the objectives of the Strategy and the communities aspirations. The proposal will utilise an existing seniors housing site to achieve the highest and best use and increase capacity in response to population growth and demographic changes. By redeveloping an existing site this will also allow other land within the Parramatta LGA to be developed into different housing types and tenures, also responding to the aspirations to provide a range of housing in the LGA.</p>

## 5.2 State Legislation

The DA's consistency and compliance with the relevant environmental planning instruments is considered in the sections below.

**Table 3 Summary of consistency with relevant strategy legislation**

Plan	Comments
<b>State Environmental Planning Policies</b>	
<i>State Environmental Planning Policy No 55 – Remediation of Land</i>	SEPP 55 aims to promote the remediation of contaminated land for the purposes of reducing the risk of harm to human health or any other aspect of the environment. A Preliminary Site Investigation has been prepared by Safe Work and Environments and is included at <b>Appendix O</b> . The investigation identifies that the site contains low potential for contaminating land activities to have occurred on the site and a Stage 2 Detailed Site Investigation is not required. The report recommends that prior to demolition works commencing an updated Hazardous Materials Survey be undertaken and a site specific Unexpected Finds Management Plan be prepared.
<i>State Environmental Planning Policy (Housing for Seniors or People with a Disability) 2004</i>	The proposed development seeks approval for seniors housing in the form of a residential care facility under the Seniors SEPP. The proposed development's consistency with the applicable controls within the Seniors SEPP is discussed below at <b>Table 4</b> .
<i>State Environmental Planning Policy No. 64 – Advertising and Signage</i>	A signage zone has been proposed as a part of the development. An assessment against SEPP 64 is provided in <b>Table 5</b> below.

### 5.2.1 State Environmental Planning Policy (Housing for Seniors or People with a Disability) 2004

The Seniors SEPP permits the development of certain types of seniors housing and sets out certain standards that override local planning provisions. **Table 4** below provides an assessment of the proposed development against the relevant clauses in the Seniors SEPP.

**Table 4 Seniors SEPP applicable planning provisions**

Provision	Response
<b>Part 2 Site Related Requirements</b>	
<i>Clause 26 - Location and access to facilities</i>	The site is located in close proximity to Westfield Parramatta. A public bus stop is located on Lansdowne Street (250m away from the pedestrian entrance to the site) providing public bus services to Merrylands via Pemulwuy. As identified in the Access Report prepared by Funktion and included at <b>Appendix J</b> , paths of travel will link the new principal entry of the building with Glebe Street via new ramps and to existing footpaths that provide access to public transport services. The proposed development is capable of complying with clause 26.
<i>Clause 27 - Bush fire prone land</i>	The site is not located on nor near bushfire prone land.
<i>Clause 28 Water and Sewer</i>	The subject site is located in an established residential area and has access to existing water and sewer services.  Necessary arrangements will be made with Sydney Water, Endeavour Energy and Jemena for connections to existing services. The proposed arrangements are capable of supporting the proposed development as discussed in the Infrastructure Services Statement prepared by JHA at <b>Appendix S</b> .
<i>Clause 30 – Site analysis</i>	A detailed site analysis plan has been prepared by Group GSA and is included at <b>Appendix A</b> . The site analysis is supported by this SEE.
<i>Clause 32 – Design of residential development</i>	The proposed development has been designed with regard to the principles of Division 2 (cl33-39).
<b>Clause 33 – Neighbourhood Amenity and Streetscape</b>	
<i>(a) recognise the desirable elements of the location's current character (or, in the case of precincts undergoing a transition, where described in local planning controls, the desired future character) so that new buildings contribute to the quality and identity of the area, and</i>	The proposed development has been designed with regard to the site's existing context, particularly the surrounding development. The building alignment recognises those of adjoining properties, while providing a more active street frontage through the inclusion of a shared zone, landscaping and large lobby area directly located off Glebe Street. The architectural style has been informed by the massing of surrounding development and therefore the proposal is considered to complement the various scales of built form within the vicinity while integrating with the existing and future residential character.
<i>(b) retain, complement and sensitively harmonise with any heritage conservation areas in the vicinity and any relevant heritage items that are identified in a local environmental plan,</i>	The site is not identified as a heritage item nor is it located within a heritage conservation area. Notwithstanding this, the proposal is proximate to a number of locally listed heritage items to the east. The proposed architectural form and setbacks will provide a sensitive response to these items and will improve the current relationship with the heritage significance of these properties as discussed in the Heritage Impact Statement at <b>Appendix Q</b> .
<i>(c) maintain reasonable neighbourhood amenity and appropriate residential character by:</i> <i>(i) providing building setbacks to reduce bulk and overshadowing, and</i> <i>(ii) using building form and siting that relates to the site's land form, and</i> <i>(iii) adopting building heights at the street frontage that are compatible in scale with adjacent development, and</i> <i>(iv) considering, where buildings are located on the boundary, the impact of the boundary walls on neighbours, and</i>	The proposed development maintains reasonable neighbourhood amenity as it: <ul style="list-style-type: none"> <li>Provides appropriate setbacks that allow for internal amenity while not resulting in any adverse impacts to surrounding residents;</li> <li>Provides a built form that is compatible with the surrounding streetscape and siting; and</li> <li>Adopts a maximum building height that is largely commensurate with the existing and future residential character and the objectives of the R4 High Density Residential zone.</li> </ul> Further discussion is provided in <b>Section 5.5</b> .
<i>(d) be designed so that the front building of the development is set back in sympathy with, but not</i>	The proposed development is setback 7m from Glebe Street. This aligns with the adjoining development at number 26 Glebe Street and number 30

<i>necessarily the same as, the existing building line, and</i>	Glebe Street. The proposed development is considered appropriate in the context of the surrounding development.
<i>e) embody planting that is in sympathy with, but not necessarily the same as, other planting in the streetscape, and</i>	The proposed development includes hard and soft landscaping to respond to the existing environment and retains majority of trees within the front setback on Glebe Street. The proposal will include 23.53% in deep soil. Landscape Plans including species type, quantity and size is included at <b>Appendix B</b> .
<i>(f) retain, wherever reasonable, major existing trees, and</i>	The proposed development seeks to retain all trees where possible however, seeks consent for the removal of 3 existing trees on the eastern boundary. Majority of trees within the front setback on Glebe Street are proposed to be retained. The proposal will include an additional 23.53% in deep soil on the front and rear setbacks.
<i>(g) be designed so that no building is constructed in a riparian zone.</i>	The site is not located near a riparian zone

#### Clause 34 – Visual and Acoustic Privacy

*The proposed development should consider the visual and acoustic privacy of neighbours in the vicinity and residents by:*

<i>(a) appropriate site planning, the location and design of windows and balconies, the use of screening devices and landscaping,</i>	The orientation and design of windows and balconies has been carefully considered in the design of the proposed development. The inclusion of landscaping and screening will ensure these rooms are fitted with appropriate visual amelioration measures to mitigate any overlooking to adjoining development. In particular the substantial area of deep soil on the rear boundary will provide a physical buffer between the site and residential dwellings adjoining the site.
<i>(b) ensuring acceptable noise levels in bedrooms of new dwellings by locating them away from driveways, parking areas and paths.</i>	Given the existing configuration of the site and with it being located in a dense urban area, the positioning of rooms away from driveways and car parking areas is unavoidable. However, it is noted that no habitable bedrooms have been located on the Ground Floor and all rooms located on Level 1-3 will be acoustically treated to ensure any noise impacts from the driveway or other sources can be avoided.

#### Clause 35 – Solar Access and Design for Climate

<i>(a) ensure adequate daylight to the main living areas of neighbours in the vicinity and residents and adequate sunlight to substantial areas of private open space, and</i>	The proposed development has been designed to maximise solar access internally for all habitable rooms, adjoining development and open space. Importantly, the proposed development has been configured to provide courtyard areas and terrace areas receive adequate sunlight during the winter solstice.
<i>(b) involve site planning, dwelling design and landscaping that reduces energy use and makes the best practicable use of natural ventilation solar heating and lighting by locating the windows of living and dining areas in a northerly direction</i>	All rooms have been designed and oriented to achieve adequate solar access while ensuring no overlooking onto adjoining development. In addition, the substantial retention of trees within the front setback will provide natural means for shading and will reduce reliance on air conditioning and the like.

#### Clause 36 – Stormwater

<i>(a) control and minimise the disturbance and impacts of stormwater runoff on adjoining properties and receiving waters by, for example, finishing driveway surfaces with semi-pervious material, minimising the width of paths and minimising paved areas,</i>	The proposed development will incorporate different stormwater measures to minimise disturbance and impacts of runoff to adjoining properties as shown on the Civil and Stormwater Plans at <b>Appendix I</b> .
<i>(b) include, where practical, on-site stormwater detention or re-use for second quality water uses.</i>	The proposal includes a new OSD tank at the rear of the car park. It is noted that the proposed development will not increase the impervious area on the site from that which currently exists. The proposed stormwater management has been designed in accordance with the PDCP 2011 requirements. Further discussion is provided at <b>Appendix I</b> .

#### Clause 37 – Crime Prevention

<i>(a) site planning that allows observation of the approaches to a dwelling entry from inside each dwelling and general observation of public areas, driveways and streets from a dwelling that adjoins any such area, driveway or street, and</i>	The proposed development aims to enhance natural surveillance to Glebe Street and Ollie Webb Reserve by orientating dwellings towards the street and increasing the activity along the streetscape, through the provision of a pedestrian oriented front setback and large lobby area. It will aim to minimise overlooking onto adjoining properties by including large deep soil
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(b) where shared entries are required, providing shared entries that serve a small number of dwellings and that are able to be locked, and	zones. The proposed seniors housing development will be designed to include secure entry points for residents and staff.
(c) providing dwellings designed to allow residents to see who approaches their dwellings without the need to open the front door.	
<b>Clause 38 – Accessibility</b>	
<i>The proposed development should:</i>	
(a) have obvious and safe pedestrian links from the site that provide access to public transport services or local facilities, and	The proposed development will include upgraded and new pedestrian paths to the site to ensure residents have a safe and accessible path of travel. Public bus services are located within 250m from the site and provide access to local services and facilities.
(b) provide attractive, yet safe, environments for pedestrians and motorists with convenient access and parking for residents and visitors.	<p>The proposed development provides distinct pedestrian and vehicular access points, improving safety across the site. The entry lobby to the proposed development has been appropriately delineated through landscaping and footpaths.</p> <p>Primary vehicular access will continue to be provided via the existing driveways adjacent to the eastern and western boundaries on the site. The western driveway will provide one way access to the car park and loading areas / ambulance bay and the eastern driveway will provide the exit point for all vehicles. The car parking arrangement will be secure and convenient and will provide direct access to the lobby area on the Ground Floor level.</p>
<b>Clause 39 – Waste Management</b>	
<i>The proposed development should be provided with waste facilities that maximise recycling by the provision of appropriate facilities</i>	Dedicated waste management rooms and facilities have been incorporated within the proposed development. Further discussion is provided at <b>Appendix N</b> .
<b>Clause 40 – Development Standards</b>	
<b>Site Size</b> – Minimum 1,000sqm	The site size complies with this provision as it is approximately 3,485.7m <sup>2</sup> .
<b>Site Frontage</b> – Minimum 20m	The site frontage complies with this provision as it is approximately 50m to Glebe Street.
<p><b>Height zones where residential flat buildings are not permitted</b></p> <p><i>If the development is proposed in a residential zone where residential flat buildings are not permitted:</i></p> <ul style="list-style-type: none"> <li>the height of all buildings in the proposed development must be 8 metres [as defined within the Seniors Housing SEPP] or less, and</li> <li>a building that is adjacent to a boundary of the site (being the site, not only of that particular development, but also of any other associated development to which this Policy applies) must be not more than 2 storeys in height, and</li> <li>a building located in the rear 25% area of the site must not exceed 1 storey in height.</li> </ul>	As residential flat buildings are permitted with consent in the R4 zone, clauses 40(4)(a), 40(4)(b) and 40(4)(c) (height controls in zones where residential flat buildings are not permitted) of the Seniors SEPP do not apply.
<b>Clause 48 – Standards that cannot be used to refuse development consent for residential care facilities</b>	
(a) <b>building height:</b> if all proposed buildings are 8 metres or less in height (and regardless of any other standard specified by another environmental planning instrument limiting development to 2 storeys),	<p>Clause 48 is not a development standard, and any non-compliance would not require a clause 4.6 variation request.</p> <p>While the proposed development will marginally exceed the maximum building height as prescribed by the PLEP 2011 of 11m, this does not result in any non-compliance and therefore a clause 4.6 variation request is not required. However, for abundant caution we have prepared a clause 4.6 variation request on a without prejudice basis which provides comprehensive justification for the proposed maximum building height. Further discussion is provided in <b>Section 6.1</b> and at <b>Appendix F</b>.</p>

(b) <b>density and scale:</b> if the density and scale of the buildings when expressed as a floor space ratio is 1:1 or less,	The proposed development will have a maximum FSR of 1.39:1. This is considered to be in line with the changing nature of the area, particularly with regard to the sites located directly to the north. Further discussion is provided at <b>Section 6.2</b> and at <b>Appendix G</b> .
(c) <b>landscaped area:</b> if a minimum of 25 square metres of landscaped area per residential care facility bed is provided,	<p>The proposed development will provide open space in the form of landscaped courtyards and terrace areas. In total the proposal will provide 2,116m<sup>2</sup> of landscaped area (approximately 60%). While this will result in approximately 584m<sup>2</sup> less than what is required under clause 48(c) it is considered suitable to suit the needs for residents on site, and with consideration of the site's locality directly opposite Ollie Webb Reserve, this non-compliance is considered negligible and can be appropriately managed.</p> <p>We note the Seniors SEPP is currently being reviewed and there is evidence that points to low usage of this quantum of landscaped area.</p>
(d) <b>parking for residents and visitors:</b> if at least the following is provided— (i) 1 parking space for each 10 beds in the residential care facility (or 1 parking space for each 15 beds if the facility provides care only for persons with dementia), and (ii) 1 parking space for each 2 persons to be employed in connection with the development and on duty at any one time, and (iii) 1 parking space suitable for an ambulance.	The proposed car parking has been provided in accordance with the Seniors SEPP. Further discussion is provided in <b>Section 6.4</b> and at <b>Appendix H</b> .

### 5.2.2 State Environmental Planning Policy No.64 – Advertising and Signage

*State Environmental Planning Policy No.64 – Advertising and Signage* (SEPP 64) applies to all signage, that under an environmental planning instrument, can be displayed with or without development consent and is visible from any public place or public reserve.

Clause 8 of SEPP 64 states that a consent authority must not grant consent for any signage application unless the consent authority is satisfied that the proposal is consistent with the objectives of the SEPP, including the assessment criteria contained in Schedule 1. An assessment against Schedule 1 – Assessment Criteria of SEPP 64 is set out in **Table 5**.

**Table 5 Assessment of signage zone against Schedule 1 of SEPP 64**

Objective	Assessment	Compliance
<b>1 Character of the Area</b>		
Is the proposal compatible with the existing or desired future character of the area or locality in which it is proposed to be located?	The proposed signage is compatible with the existing locality and does not adversely impact on any residential dwellings located on Glebe Street. The proposed signage zone is simple in nature and will be affixed to a backing panel at the entry point to the site, appropriately communicating the operations of the site as a RACF.	Compliant
Is the proposal consistent with a particular theme for outdoor advertising in the area or locality?	The proposed freestanding pylon sign will be installed on a temporary basis only and is considered appropriately located and designed to communicate the relevant details to the surrounding community. The proposed signage will not detract from the surrounding area and rather.	Compliant
<b>2 Special Areas</b>		
Does the proposal detract from the amenity or visual quality of any environmentally sensitive areas, heritage areas, natural or other conservation areas, open space areas, waterways, rural landscapes or residential areas?	<p>The proposed signage has been designed to integrate with the sites landscaped setting and the new architectural design of the facility, as well as provide relevant information during the construction phase. It has also had regard to the existing residential development located along Glebe Street.</p> <p>The signage zone is not situated in a heritage conservation area or nearby to any heritage items. While the signs are located in a residential setting, the materiality, sign and indicative content will not detract from the amenity of the area.</p>	Compliant

Objective	Assessment	Compliance
<b>3 Views and Vistas</b>		
Does the proposal obscure or compromise important views?	<p>The proposed signage is integrated with the main entrance landscaped design and will there not result in any obstruction of views, and the location and content of signage will not otherwise compromise important views within the area.</p> <p>The proposed freestanding pylon sign will be installed on a temporary basis only and sits between existing large canopy trees and will not protrude above the trees' canopies.</p>	Compliant
Does the proposal dominate the skyline and reduce the quality of vistas?	<p>The proposed signage is appropriate to the scale of the site and its surroundings and intended use as a business identification sign.</p> <p>The proposed freestanding pylon sign sits beneath the canopy of the adjacent trees and will not dominate or reduce the quality of vistas.</p>	Compliant
Does the proposal respect the viewing rights of other advertisers?	The proposal does not block any other signage in the area.	Compliant
<b>4 Streetscape, setting or landscape</b>		
Is the scale, proportion and form of the proposal appropriate for the streetscape, setting or landscape?	<p>The scale, proportion and form of the proposed signage is consistent with the setting of the facility, being at the entry point, but of a scale that does not detract from the amenity of the residential environment.</p> <p>The scale of the proposed freestanding pylon sign is appropriate for the streetscape during the construction phase given that it will provide appropriate identification and relevant details and will sit adjacent to temporary hoarding / safety fencing. It will also sit beneath the canopy of existing trees and is therefore appropriate for the streetscape and landscape setting.</p>	Compliant
Does the proposal contribute to the visual interest of the streetscape, setting or landscape?	The proposed signage contributes to the visual interest of the streetscape by contributing to the identification and recognition of the service offering on the site and provides relevant details during the construction phase.	Compliant
Does the proposal reduce clutter by rationalising and simplifying existing advertising?	The proposed signage does not reduce visual clutter.	Compliant
Does the proposal screen unsightliness?	The proposed signage does not screen unsightliness.	Compliant
Does the proposal protrude above buildings, structures or tree canopies in the area or locality?	The proposed signage does not protrude above buildings, structures or tree canopies.	Compliant
Does the proposal require ongoing vegetation management?	The proposed signage will not require ongoing vegetation management.	Compliant
<b>5 Site and Building</b>		
Is the proposal compatible with the scale, proportion and other characteristics of the site or building, or both, on which the proposed signage is to be located?	The proposed signage has been designed to be fully compatible with the streetscape, landscape design and architectural design of the new facility.	Compliant
Does the proposal respect important features of the site or building, or both?	The proposed signage has been located in an appropriate location to respect the surrounding residential development and landscaped setting.	Compliant
Does the proposal show innovation and imagination in its relationship to the site or building, or both?	The proposed signage has been fully integrated with the proposed architectural and landscaped design of the setting.	Compliant

Objective	Assessment	Compliance
<b>6 Associated devices and logos with advertisements and advertising structures</b>		
Have any safety devices, platforms, lighting devices or logos been designed as an integral part of the signage or structure on which it is to be displayed?	No safety devices, platforms, lighting devices or logos are incorporated as an integral part of the signage zone.	N/A
<b>7 Illumination</b>		
Would illumination result in unacceptable glare?	The proposed signage will include illumination that will not result in any unacceptable glare. The illumination will not affect the safety of pedestrians, vehicles or aircraft.	Compliant
Would illumination affect safety for pedestrians, vehicles or aircraft?		Compliant
Would illumination detract from the amenity of any residence or other form of accommodation?	The proposed illumination of the signage will not detract from the amenity of any residence proximate to the site.	Compliant
Can the intensity of the illumination be adjusted, if necessary?	The intensity of the illumination may be adjusted.	Compliant
Is the illumination subject to a curfew?	The illumination is not subject to curfew.	Compliant
<b>8 Safety</b>		
Would the proposal reduce safety for any public road?	The proposed business identification sign will replace the existing sign located on Glebe Street. The proposed will not impact the safety on Glebe Street. The proposed signage will be located at ground level and affixed to a timber backing panel. The proposed signage will not distract from essential sight lines for pedestrians and cyclists.  The temporary freestanding pylon sign will sit beneath the tree canopy and will be installed during the construction phase only. The sign will provide appropriate details of the service during the construction phase and will not impact vehicle road safety or pedestrian cyclist safety.	Compliant
Would the proposal reduce safety for pedestrians/cyclists?		Compliant
Would the proposal reduce the safety for pedestrians, particularly children, by obscuring sightlines from public areas?	The proposal will not reduce safety for pedestrians or children as it does not block any significant sightlines from public areas of key importance.	Compliant

### 5.3 Parramatta Local Environmental Plan 2011

**Table 6 Key applicable planning provisions under the PLEP 2012**

Clause	Provision / Standard	Proposal
Clause 2.3 Zone Objectives and Land Use Table	R4 High Density Residential	The site is zoned R4 High Density Residential. Development for the purposes of seniors housing is permissible in the R4 zone.
Clause 4.3 – Height of Buildings	11m	<p>The proposed development has a maximum building height of 12.9m. We note that in our opinion a clause 4.6 variation is not necessary in this instance on account of the proposed development subject of the variation being pursuant to the Seniors SEPP and it prevailing the standards of the PLEP 2011 in the extent of an inconsistency.</p> <p>However, for abundant caution we have prepared a clause 4.6 variation request on a without prejudice basis which provides comprehensive justification for the proposed maximum building height. Further discussion is provided in <b>Section 6.1</b> and at <b>Appendix F</b>.</p>
Clause 4.4 – Floor Space Ratio	0.8:1	<p>The proposed development has a maximum FSR of 1.39:1 measured in accordance with the Seniors SEPP PLEP 2011.</p> <p>We note that in our opinion a clause 4.6 variation is not necessary in this instance on account of the proposed development subject of the variation being pursuant to the Seniors SEPP and it prevailing the standards of the PLEP 2011 in the extent of an inconsistency.</p> <p>However, for abundant caution we have prepared a clause 4.6 variation request on a without prejudice basis which provides comprehensive justification for the proposed maximum FSR. Further discussion is provided at <b>Section 6.2</b> and at <b>Appendix G</b>.</p>
Clause 7.1 – Acid Sulfate Soils	Class 5	The site is not located in close proximity to any land identified as class 1, 2, 3 or 4. No further assessment is required.
Clause 5.10 – Heritage	The site is not heritage listed nor is it located within a heritage conservation area. However, the site is proximate to a number of local heritage items. A Heritage Impact Statement has been prepared by John Oultrom Heritage and Design and is included at <b>Appendix Q</b> . Further assessment is provided at <b>Section 6.11</b> .	
Clause 5.10.8 Aboriginal Places of Heritage Significance	The site is identified as being of low significance by Council's Aboriginal heritage Sensitivity Database.	
Flood Planning	The site is not identified as being flood prone.	

### 5.4 Section 7.11 Development Contributions

All development under Section 7.11 must pay development contributions in accordance with the *Parramatta Section 94A Development Contributions Plan (Amendment No. 5)*, unless it is specified types of development, including being classified as social or affordable housing.

It is requested that Council not apply development contributions to the proposal as per Part 3.6 of the document, where the proposed development is for the purposes of providing affordable (aged) housing. A formal letter of request is included at **Appendix U** which also provides greater detail on the justification for this request.

Residential Care Places have conditions imposed under section 14.5 of the *Aged Care Act*. One condition is that residential aged care places must be used to provide care to supported residents to at least the resident support ratio for the region in which a service is located. 'Supported Residents' are eligible for concessional beds who pay low or no accommodation bond or charge.

The current supported resident ratio for the region that Brentwood is located is 29.8%. However, the actual supported resident ratio at Brentwood is currently 78.3%, far above the minimum requirement. The proposed redevelopment of the site will achieve a similar ratio to continue to service residents of low financial means.

Development Contributions go towards the servicing and installation of infrastructure including community facilities, open space and recreation, public domain, natural environment, drainage, roads and shared paths. As the development will provide infrastructure and services on site, and the fact that residents will not likely draw upon Council's services, it is requested that council do not apply the provisions of their s94A contributions plan under section 3.1 of the plan on the basis that the project is providing affordable (aged) housing.

## 5.5 Parramatta Development Control Plan 2011

The proposed development is consistent with the objectives of the PDCP 2011 and the relevant controls. As required under Section 4.15(3A) of the EP&A Act, a consent authority is required to apply DCP provisions flexibly and allow reasonable alternative solutions that achieve the objects of those standards. In lieu of the objectives and controls in the PDCP 2011 for the proposed use as seniors housing, the key aspects are considered in **Table 6** and where alternate solutions are proposed, they are discussed in the following sections of this environmental assessment.

There are no direct provisions in the DCP for a RACF building, which is a very specific form of development that is an amalgam of residential and hospital-like development. However, we have assessed it under the provisions of the DCP for residential flat buildings in the following.

**Table 7 Parramatta Development Control Plan 2011**

Control	Proposal	Assessment location
<b>Section 3.1 Preliminary Building Envelope – Residential Flat Buildings</b>		
Height Control	The maximum building height under the PLEP is 11m. The proposed development has a maximum building height of 12.9m. This is considered suitable in the context of the zone and recent development approvals in the area. The proposal also adopts substantial setbacks to adjoining development to reduce overshadowing, overlooking, and ameliorate any other privacy concerns. While a clause 4.6 variation request to the maximum building height control is not considered applicable (given the proposal is pursuant to the Seniors SEPP), Ethos Urban has prepared a clause 4.6 which is included at <b>Appendix F</b> .	<b>Section 6.1 Appendix F</b>
FSR	The maximum FSR under the PLEP 2011 is 0.8:1. The proposed development has a maximum FSR of 1.39:1 measured in accordance with the Seniors SEPP. While a clause 4.6 variation request to the maximum FSR control is not considered applicable (given the proposal is pursuant to the Seniors SEPP), Ethos Urban has prepared a clause 4.6 which is included at <b>Appendix G</b> .	<b>Section 6.2 Appendix G</b>
Minimum site frontage	The proposed development has a site frontage of 50m. The proposed development complies with this provision.	<b>Appendix A</b>
Front setback	The proposed development adopts a setback to Glebe Street of 7m. The proposed development complies with this provision.	<b>Appendix A</b>
Side setback	The proposed development adopts a setback of 4.5m to the eastern and western side boundaries. This is considered suitable in the context of the site's configuration and the privacy measures that have been integrated into the development in order to ensure any privacy concerns are minimised to adjoining development. The building setbacks are further discussed in <b>Section 6.3</b> .	<b>Section 6.3 Appendix A</b>
Rear Setback	The proposed development has a setback of 10.3m to the rear boundary. The proposed development complies.	<b>Section 6.3 Appendix A</b>
Deep Soil Zone	When calculated in accordance with the PDCP 2011 provisions, the proposed development will provide 23.53% deep soil, equating to an area of 820.2m <sup>2</sup> . While the proposal will provide marginally less deep soil than what is required under the DCP, it is considered that the retention of existing trees within the front setback and addition of more deep soil planting than what currently exists will provide an overall improvement to the site and an optimal design outcome. Further, the form of development, being a RACF has different site requirements than a residential flat building.	<b>Appendix B</b>
Landscape Area	The proposal will provide a minimum 33.5% of landscaped area. This equates to 1,168m <sup>2</sup> .	<b>Appendix B</b>



Control	Proposal	Assessment location
	<p>While the proposal will result in marginally less landscaped area than what is required (by 225.68m<sup>2</sup>) this is considered negligible in the context of the site's location and its configuration in providing greater and more useable landscaped area than what currently exists. As well, the proposal is located directly opposite Ollie Webb Reserve which provides substantial open space in an accessible location.</p> <p>In addition, the requirements of a RACF are far lesser than a residential flat building when considering outdoor landscaped amenity.</p>	
<b>Section 3.2 Building Elements</b>		
3.2.1 Building Form and Massing	<p>The proposed development has been designed to incorporate an appropriate transition in built form by modulating the building lines to ensure there is no adverse impact to adjoining residential development.</p> <p>Through the adoption of an 'H' configuration with two wings located off a deep central core zone further setback from the eastern and western boundaries, this assists to reduce any perceived bulk and scale. As shown in the Architectural Plans included at <b>Appendix A</b>, the proposal has adopted privacy measures and substantial deep soil planting to provide physical visual and acoustic buffers.</p> <p>The building massing proposed has also had regard to the changing nature of the area where a number of developments have been approved with a variation to the maximum building height and FSR controls on the Great Western Highway directly north of the site and along Marsden Street. Given that the proposal is located on a sloping topography it is considered that the proposed height and massing is commensurate with a built form envisioned by the objectives of the R4 High Density Residential zone and would not result in any overshadowing or privacy impacts to adjoining development.</p>	<b>Section 6.3 Appendix A</b>
3.2.2 Building Facades and Articulation	<p>As discussed, the proposed development will be constructed using various materials and finishes to create a contemporary, high quality development that is compatible with the surrounding built form, while providing visual interest and complementing the character of the adjoining development and streetscape. The proposed material palette provides a range of different fenestration patterns, while breaking down the façade detail into elements that is similar to residential properties in the area. Specifically, the proposal includes:</p> <ul style="list-style-type: none"> <li>• Extruded Brick</li> <li>• Metal Appearance Cladding</li> <li>• Acrylic Render</li> <li>• Glass Balustrade</li> <li>• Aluminium Extrusion</li> <li>• Outdoor Paint</li> </ul> <p>The roof form adopts a pitch formation commensurate with the surrounding context.</p>	<b>Appendix A</b>
3.2.3 Roof Design		
3.2.4 Energy Efficient Design	The proposed development has been designed with regard to the BASIX design principles and includes properties incorporated into the development to improve energy efficiency.	-
3.2.5 Streetscape	<p>The proposed development will improve the site's streetscape appearance to Glebe Street. Through the retention of the existing mature trees fronting Glebe Street, and incorporation of a pedestrian friendly ground plane design, the proposal assists in improving the pedestrian amenity along Glebe Street and most importantly provides a setback that is consistent with adjoining development.</p> <p>Existing vehicular access points will be maintained and the flow of traffic will be directed to access the site from the western driveway and exit using the eastern driveway. The proposed development includes the reconfiguration of the existing at-grade car park within the front setback to create a more pedestrian friendly environment and the inclusion of a shared zone at the site's entry. A new car park is proposed at the ground level to provide 23 car spaces. This will further enhance the site's presence to Glebe Street, where the carport will be discrete from the public domain being located behind the building line and overhang of the building above.</p> <p>The proposal complies with this provision.</p>	<b>Appendix A</b>

Control	Proposal	Assessment location
<b>Section 3.3 Environmental Amenity</b>		
3.3.1 Landscaping	<p>The proposed landscape design has been integrated into the development, with the retention of existing trees along Glebe Street and the introduction of substantial deep soil zones within the rear setback.</p> <p>These elements as well as the introduction of new courtyards and terrace areas will enhance the visual setting for residents and neighbours alike and accentuate the design quality of the built form.</p> <p>The retention of existing trees and introduction of new trees will allow for a physical buffer between the site and adjoining development and will create a substantial screening element to ensure any privacy concerns are mitigated.</p>	<b>Appendix B</b>
3.3.2 Private and Communal Open Space	<p>The proposed development has been designed with regard to the Seniors SEPP control where the provisions override the provisions of the PDCP 2011.</p> <p>The proposal includes 2,116m<sup>2</sup> of landscaped area and open space across the Ground Level and Levels 1-3. While this results in a minor non-compliance with the provisions of the Seniors SEPP, it is considered marginal in the context of the site's configuration and its location directly opposite Ollie Webb Reserve.</p> <p>Due to the nature of the development being seniors housing and its residents, it is not considered appropriate to provide a communal open space landscaped area on the rooftop given the access requirements as well as possible privacy visual and acoustic privacy concerns.</p>	<b>Appendix A</b>
3.3.3 Visual and Acoustic Privacy	<p>As shown on the Architectural Plans, the habitable rooms have been oriented to face north-south rather than east-west where the site adjoins other residential development. All windows are oriented in this direction to allow residents to overlook areas of open space and not adjoining development.</p> <p>Where habitable rooms face north, a setback of 10.3m has been adopted and substantial deep soil planting within this zone is proposed to provide a physical buffer between development. This will ensure there is little to no overlooking to adjoining development and is in line with the relevant ADG controls (although not applicable to seniors housing) as well as the setback provisions of the PDCP 2011.</p>	<b>Appendix A</b>
3.3.4 Acoustic Amenity	<p>All habitable rooms will be acoustically treated to ensure there is appropriate mitigation to any noise generating uses.</p> <p>An Acoustic Impact Assessment has been prepared to detail the appropriate acoustic measures that will be included as part of the proposed development, including glazed windows and doors and dimensions of rooms. The Acoustic Impact Statement confirms that provided the proposed development includes the recommendations of the assessment, the internal noise levels will comply with the relevant requirements of the DCP, <i>Australian Standards</i> and the <i>NSW Environmental Protection Authority – Noise Policy for Industry</i> (NPII) 2017.</p> <p>Detailed acoustic control measures for the plant servicing the development will be determined at the Construction Certificate stage.</p>	<b>Section 6.7 Appendix K</b>
3.3.5 Solar Access and Cross Ventilation	<p>The habitable rooms have been oriented so as to optimise solar access to residents while respecting the amenity of adjoining development and to ensure minimal overlooking. The separation distance between both wings will allow for appropriate solar access. All rooms will be fitted with an external window to provide direct light and ventilation. No window is located opposite an opposing wall.</p> <p>The floor to ceiling height across the development is 3.3m.</p> <p>Building setbacks have been established in accordance with the relevant PDCP 2011 and ADG provisions.</p> <p>The proposed development complies with this provision.</p>	<b>Appendix A</b>
3.3.6 Water Sensitive Urban Design	<p>The proposed development reduces the impervious area on the site by approximately 13%. Water sensitive urban design provisions have been provided as part of the proposal in order to protect downstream receiving water bodies. The proposed development meets and exceeds the target objectives as specified in the PDCP 2011. Further discussion is provided in <b>Appendix I</b>.</p>	<b>Appendix I</b>
3.3.7 Waste Management	<p>An Operational Waste Management Plan and Construction Waste Management Plan has been prepared by UFD. The plans assess the methods to reduce quantity of waste on site and the disposal of waste.</p>	<b>Section 6.9 Section 6.10 Appendix N</b>



Control	Proposal	Assessment location
<b>Section 3.4 Social Amenity</b>		
3.4.2 Access for People with Disabilities	The proposed development has been designed with regard to the access and design controls of the Seniors SEPP as well as the relevant standards for RACF developments. The proposed development is supported by a Access Statement.	<b>Appendix J</b>
3.4.4 – Safety and Security	<p>The proposed development includes a design that improves the presence of the site's interface with the public domain and includes passive elements that provide opportunities for passive surveillance to the public and residents. The proposal includes clear lines of site within and around the entry point and clearly delineates vehicular and pedestrian access.</p> <p>Each level is accessible via a central lift core. Amenities and facilities including lounge and dining areas are located on each floor. Access to the facility will be controlled via key access control for staff and visitors will be triaged through the lobby reception area.</p> <p>The proposed facility will improve the site's existing interface with the public domain providing a development that vastly improves pedestrian safety from that which currently exists.</p>	-
3.4.5 – Housing Diversity and Choice	The proposed development utilises an existing seniors housing development to upgrade the facility and increase capacity on the site to suit population needs and demographic changes. This will create opportunities for more residents to age in place and allow for other vacant sites or infill development to occur in the Parramatta LGA and provide an increased range of household types and tenures.	-
<b>Section 3.5 Heritage</b>		
3.5.1 Heritage	<p>The site is not listed as a heritage item nor is it located in a heritage conservation area. However, the site is proximate to various locally listed heritage items.</p> <p>As detailed in the Heritage Impact Statement prepared by John Oultram Heritage and Design, the proposed development has adopted a well considered response and is of a scale that is well articulated providing an enhanced presentation to the streetscape with limited and acceptable impact on heritage items within the vicinity.</p>	<b>Section 6.8 Appendix Q</b>
<b>Section 3.6 Movement and Circulation</b>		
3.6.2 Parking and Vehicular Access	<p>The proposed development will utilise the existing ingress and egress access points on the western and eastern boundaries of the site. The western driveway will provide access to the car park area on the Ground Floor which will provide 23 car spaces in accordance with the Seniors SEPP provisions for RACFs. It will also provide access for an ambulance to the ambulance bay adjacent to the lobby entrance and the loading and services bay towards the north-eastern boundary of the site adjacent to the back of house areas.</p> <p>All vehicles will exit the site in a forward direction from the eastern driveway.</p> <p>Clear delineation between the car park, ambulance bay and loading / services bay will be made to ensure there is no conflict between staff / visitors or services personnel. Car parking has been designed in accordance with the Australian Standards.</p>	<b>Section 6.6 Appendix H</b>
3.6.3 Accessibility and Connectivity	The proposed development for the purposes of seniors housing in the form of a RACF has been assessed by an access consultant in accordance with the relevant accessibility standards for seniors and the Seniors SEPP. The assessment confirms the proposed development is capable of complying with the relevant BCA and Australian Standards subject to further detailed design.	<b>Appendix J</b>

### 5.5.1 Section 5.5 – Signage

The table below details the proposed signage zone's compliance with the relevant provisions contained under Section 5.5 of the PDCP 2011.

**Table 8 Assessment of compliance with relevant signage provisions**

Provisions	Assessment	Compliance
<b>General Provisions</b>		
Signs are to be sited and designed so that they do not adversely impact on the amenity of the streetscape and the surrounding locality, in particular signs are not to dominate or obscure other signs or result in visual clutter.	The proposed signage does not adversely impact on the amenity of the streetscape and the surrounding locality	Compliant
Signs are to be compatible with the design, scale and architectural character of the building or site on which they are to be placed.	The signage is compatible with the design, scale and architectural character of the future development and the surrounding locality.	Compliant
Structures supporting signs should be of a high aesthetic appearance and not impact on the visual amenity of the locality.	<p>The proposed signage materiality has been determined based on the high quality architectural design and landscaped setting. The proposed signage will contribute to the visual amenity of the locality.</p> <p>The proposed freestanding pylon sign will provide appropriate details of the project during the construction phase and will not detract from the visual amenity of the surrounding locality.</p>	Compliant
Materials used should be durable, fade proof and of a high aesthetic quality.	The proposed materiality will be durable and of a high aesthetic quality as shown on the Architectural Plans at <b>Appendix A</b> .	Compliant
<p>Sign content is to relate directly to a use, business or activity carried out on or associated with the building or site on which the sign is to be placed, or to within 400m of the site, except where the sign:</p> <ul style="list-style-type: none"> <li>is incorporated with a bus shelter, home kiosk, telephone booth,</li> <li>street furniture and the like, or</li> <li>is in conjunction with the provision of public infrastructure, or</li> <li>incorporates sponsorship acknowledgement.</li> </ul>	The signage is located on the site and relates to the operation of the site as a RACF.	Compliant
Sponsorship acknowledgement will be limited to words related to the sponsoring company's name, and the sponsor's logo, provided it does not exceed 5% of the area of the sign.	The signage will not include any sponsorship acknowledgement.	Compliant
The language of signs is to be accessible to the wider population.	The signage content will be accessible for the wider population.	Compliant
Signs and their supporting structures are to be structurally sound and constructed to ensure pedestrian and traffic safety.	The signage and content will be structurally sound.	Compliant
<p>Signs and their supporting structures should not be:</p> <ul style="list-style-type: none"> <li>hazardous to passers-by and for traffic safety</li> <li>located so as to obscure a driver's or pedestrian's view of road or rail vehicles, pedestrians or features of the road, railway or footpath highly illuminated so as to cause discomfort to, or inhibit vision of drivers or pedestrians</li> <li>mistaken as an official traffic sign and should not distract a drivers attention or be confused with traffic signal instructions.</li> </ul>	The signage is set back from Glebe Street and will not be hazardous to passers by and traffic. The business identification sign will replace the existing sign and the proposed temporary freestanding pylon sign will provide appropriate details of the site during the construction phase.	Compliant
The erection of any sign must comply with the applicable requirements of the Building Code of Australia	Noted.	Compliant

Provisions	Assessment	Compliance
Illuminated signs are not to detract from the architecture of the supporting building during daylight	Noted.	Compliant
Illuminated signs are to be energy efficient	Noted.	Compliant
In considering applications for new signs, the consent authority must have regard to the number of existing signs on the site or the number of signs on a new building and in its vicinity and whether the cumulative impact gives rise to visual clutter.	Noted.	Compliant
A curfew may be imposed on the operation of illuminated signs where continuous illumination may impact adversely on the amenity of residential buildings, serviced apartments or other visitor accommodation, or have adverse environmental effects.	Noted.	Compliant
External lighting of signs is to be downward pointing and focused directly on the sign and is to prevent or minimise the escape of light beyond the sign.	Noted.	Compliant
<b>Residential Zones</b>		
Signs in residential zones are not to detract from the residential amenity of the locality.	The proposed signage is located in a position where it will not detract from residential amenity	Compliant
Signage is restricted to one business identification sign identifying the office of a professional person, or a home occupation business that meets the exempt development requirements.	The proposed business identification sign and freestanding pylon sign will be the only signs on the site.	Compliant

## 6.0 Planning Assessment

### 6.1 Building Height

#### Statutory Context

The Height of Buildings development standard under clause 4.3 in the PLEP 2011 is 11m.

For the purposes of development pursuant to the Seniors SEPP, height in relation to a building, means “*the distance measured vertically from any point on the ceiling of the topmost floor of the building to the ground level immediately below that point*”.

Clause 48 of the Seniors SEPP provides that a consent authority must not refuse consent to a development application made pursuant to that Chapter for the carrying out of development for the purpose of a residential care facility on building height: if all proposed buildings are 8 metres or less in height (and regardless of any other standard specified by another environmental planning instrument limiting development to 2 storeys). It is further noted that the provisions contained in clause 48 of the Seniors SEPP are not development standards and the development standards provided under clause 40 of the Seniors SEPP relating to building height, do not relate to development where residential flat buildings are permitted.

As the PLEP 2011 imposes a height control and height definition that is different to the interpretation under the Seniors SEPP, there is an inconsistency between the two policies, and the Seniors SEPP prevails by virtue of cl 5(3) of the Seniors SEPP, which provides that:

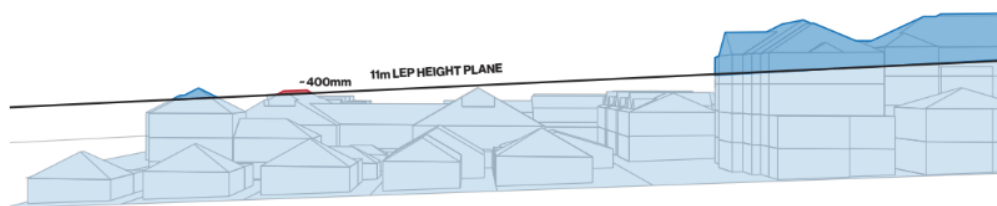
*If this Policy is inconsistent with any other environmental planning instrument, made before or after this Policy, this Policy prevails to the extent of the inconsistency.*

The proposed development deviates from the maximum building height control of 11m as specified in the PLEP 2011 by 1.9m.

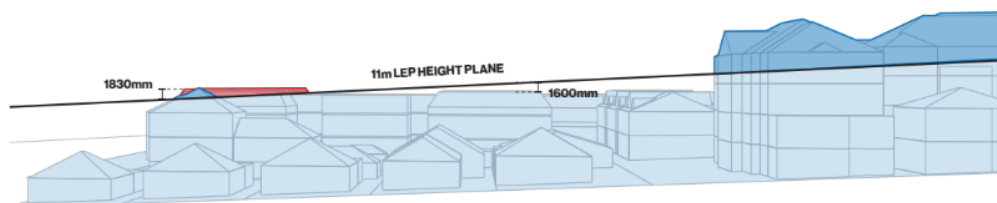
Therefore, despite exceeding the maximum building height control of 11m, in this instance the development standards in the PLEP 2011 do not represent a statutory standard that requires a clause 4.6 variation request and we note that the standards in clause 48 do not preclude the granting of development consent, but rather if the standards are breached then the proposal would require a merit assessment under Section 4.15 of the EP&A Act. This is consistent with the approach taken by *Eastern Suburbs Leagues Club Pty v Waverley Council* [2019] NSWLEC 130, Moore J (in Class 1). However, for abundant caution a clause 4.6 variation request has been prepared to comprehensively justify the proposed maximum building height for the RACF and is included at **Appendix F**.

#### Proposed Variation to LEP HOB Standard

As shown in **Figure 16** below, the proposal results in a minor 1.9m exceedance to the maximum building height control in the southern portion of the site where the site slopes down from the Great Western Highway to Glebe Street. While the roof will exceed the maximum permissible height control under the PLEP 2011, the proposed recessive mansard roof form will conceal the third storey within the building height whilst ensuring a more sympathetic presence along the Glebe Street elevation.

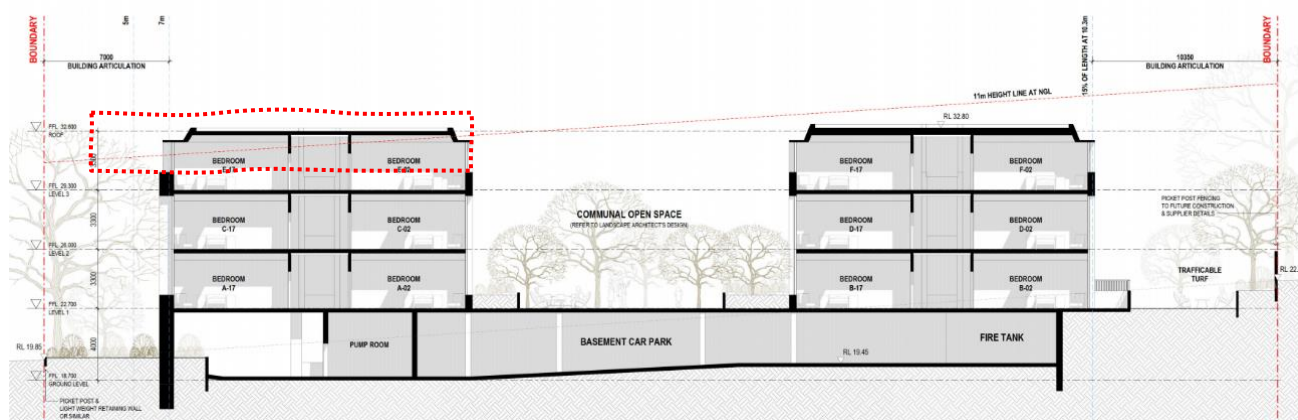


Existing



**Proposed**

**Figure 17 Plan illustrating the proposal as viewed from Marsden Street facing west**  
Source: Group GSA



**Figure 18** Section illustrating the 11m height exceedance in the southern portion fronting Glebe Street  
Source: Group GSA

## Assessment

While the height control prescribed in the PLEP 2011 cannot be taken as the jurisdictional bar for development consent or refusal, in accordance with section 4.15 of the EP&A Act, we have assessed the proposed height on its merit with specific regards to the LEP standard and its objectives

The objectives for the maximum height of buildings under clause 4.3 of the PLEP 2011 prescribe that:

- (a) to nominate heights that will provide a transition in built form and land use intensity within the area covered by this Plan,*
- (b) to minimise visual impact, disruption of views, loss of privacy and loss of solar access to existing development,*
- (c) to require the height of future buildings to have regard to heritage sites and their settings,*
- (d) to ensure the preservation of historic views,*
- (e) to reinforce and respect the existing character and scale of low density residential areas,*

*(f) to maintain satisfactory sky exposure and daylight to existing buildings within commercial centres, to the sides and rear of tower forms and to key areas of the public domain, including parks, streets and lanes.*

The proposal and in particular the proposed exceedance does not result in any departure from the objectives of the standard, where the proposed height is considered to reflect the scale of development within the area and the objectives of the R4 High Density Residential zone.

The proposal is considered to be in accordance with the maximum building height development standard under the PLEP 2011 as follows:

- The objectives of the standard are achieved, particularly with the proposal being compatible within its context given the changing nature of development within the vicinity of the site and approvals for variations to development standards along the Great Western Highway to the immediate north of the site;
- The proposed development has been designed to minimise overshadowing on the surrounding locality by considering the minimum setback controls as provided by the Apartment Design Guide (ADG) and the PDCP 2011. Importantly, the proposed non-compliance of 1.9m will still allow the adjoining properties to receive the minimum requirement of 3 hours solar access during the early to mid morning period;
- The proposed development will not result in any significant impacts to any heritage items within the vicinity and the development provides a well-considered response to the provision of seniors housing;
- The proposal is for the redevelopment of the site and while it will result in a minor height noncompliance this will not impact any historic views to or from the site;
- The proposed development will add to the mixed character of the area through providing housing diversity, whilst respecting the neighbouring developments and open space; and
- The proposal has had regard to adjoining development adopting a modulated built form that will increase setbacks to neighbouring residential properties and allow for appropriate solar access.

Accordingly, the proposed height is considered appropriate as it reflects the general scale of that envisaged under the PLEP 2011 and approved development within the vicinity, creating an appropriate built form transition whilst meeting the objectives of the control.

Further, as highlighted in **Figure 16** above, as a result of the proposed RACF use, which incorporates greater building setbacks and privacy measures from that which currently exist, as well as a high quality architectural design including consideration of the development's presence along Glebe Street, the proposed height exceedance will be negligible in the context of the proposal's overall upgrade to the site and significant public benefits. Any other associated impacts from the height exceedance are further mitigated through the proposal's specific design and landscape strategy.

Further assessment is provided in the clause 4.6 variation request prepared by Ethos Urban at **Appendix F**.

## 6.2 Floor Space Ratio

### Statutory Context

The maximum floor space ratio standard (FSR) under the PLEP for the site is 0.8:1. As discussed above, the proposed development is pursuant to clause 48 of the Seniors SEPP and where there are any inconsistencies between two policies, the Seniors SEPP prevails.

While the proposal exceeds the maximum LEP FSR control, this does not represent a statutory standard that requires a clause 4.6 variation request and clause 48 of the Seniors SEPP does not preclude the granting of development consent, but rather provides that a merit assessment be undertaken. However, for abundant caution a clause 4.6 variation request has been prepared to comprehensively justify the proposed maximum FSR for the RACF and is included at **Appendix G**.

## Proposed Variation to LEP FSR Standard

The proposed development seeks consent for a total GFA of 4,869.7m<sup>2</sup> calculated in accordance with the Seniors SEPP or 4,850.2m<sup>2</sup> calculated in accordance with the PLEP 2011. The proposed development will have an FSR of 1.39:1.

## Merit Assessment

The objectives for the maximum FSR under clause 4.4 of the PLEP 2011 prescribe that:

- (a) to ensure development is compatible with the bulk, scale and character of existing and future surrounding development,*
- (b) to provide for a built form that is compatible with the role of town and major centres.*

The clause 4.6 variation request included at **Appendix G** demonstrates that, notwithstanding the non-compliance with the FSR development standard:

- There are sufficient environmental planning grounds to justify the variation as the proposed FSR is compatible with its context, given the varying nature of buildings within the vicinity, the topographical change and configuration of the site;
- The site provides a substantial public benefit through appropriate building articulation and the provision of an upgraded seniors housing development that responds to the contemporary standards of aged care and increases capacity on an existing aged care site that responds to the growing population and ageing demographic; and
- The proposal is in the public interest since it is consistent with the objectives of the R4 High Density Residential zone and provides multiple public benefits.

Therefore, the DA may be approved with the variation as proposed in accordance with the flexibility allowed under clause 4.6 of the PLEP 2011.

## 6.3 Setbacks

The proposed development has been formulated in response to the site's context, underlying planning provisions and the desire to optimise amenity for adjoining residential development and future residents.

The bulk and scale of the proposed development is suitable based on its context, mindful that there are no underlying controls for bulk and scale on the site and for RACFs in the PDGP 2011. The development has adopted those controls that apply to residential flat buildings in the R4 High Density Residential zone as specified in the PDGP 2011. The proposed development has also been designed with regard to the controls provided in the Apartment Design Guide (ADG) – despite not applying to this land use.

By adopting these controls, the proposal ensures generous setbacks to all boundaries on the site as follows:

- 7m to Glebe Street;
- 4.5m to the east and west setback; and
- 10.3m to the rear setback (reflecting the 15% length of the site).

The proposal further minimises any perceived bulk and scale from the adjoining development through its modulated configuration adopting an 'H' formation. This form results in a larger setback from the adjoining development at the centre of the site allowing for appropriate amenity to be achieved.

Further, the use of varied materiality and a specific landscape design that has sought to maintain majority of trees within the site ensures the building massing is broken up and all setbacks within the development comprise appropriate landscaping to provide a buffer between adjoining developments.



## 6.4 Amenity

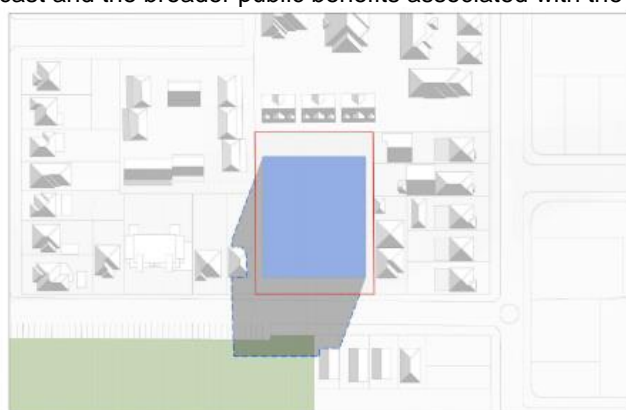
### 6.4.1 Overshadowing

Overshadowing analysis has been undertaken by Group GSA and is included at **Appendix A** to demonstrate the potential impact of the proposed development on adjoining properties.

The analysis demonstrates that the proposed development will result in some increase in shadow from that which currently exists. As demonstrated on the shadow diagrams, the future built form will cast majority of its shadow onto Glebe Street due to the orientation of the site and while the proposal will result in some additional shadow onto Ollie Webb Reserve, this is only marginal and will only occur during the early morning period during lower times of use. The overshadowing to the reserve during these periods will be approximately 401.5m<sup>2</sup> of the total area at 9am (0.68%) and 27.7m<sup>2</sup> (0.047%) at 10am. All of the reserve will continue to receive adequate solar access during the winter solstice from 10am onwards.

While it is noted that the proposal will result in an increase in overshadowing from that existing, the existing development on the eastern boundary of the site will continue to receive 3 hours solar access to habitable rooms and private open space in the early morning to midday period. It is further noted that the proposal adopts a greater setback to these properties than that prescribed under the PDCP 2011 and as illustrated in **Figure 18** and the Architectural Plans at **Appendix A**, the proposed development will result in marginally less overshadowing than what would be cast by a compliant built form.

Given the dense urban context and the site's orientation it is noted that any additional overshadowing is unavoidable. However, the impact of this is considered negligible when looking at the portions of additional shadow cast and the broader public benefits associated with the development.



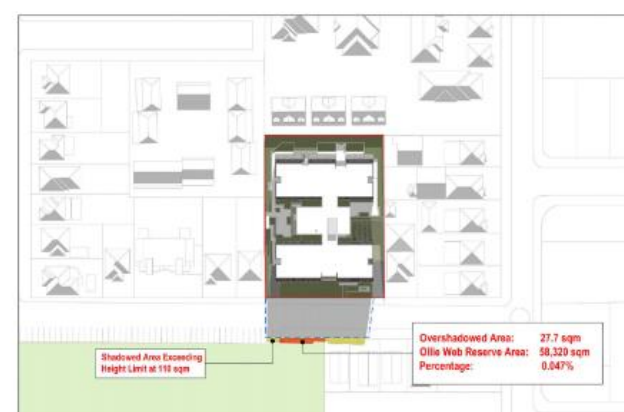
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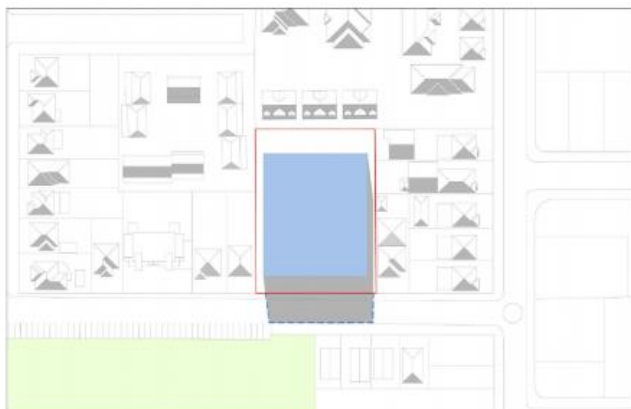


3 COMPLYING ENVELOPE - 10:00 AM - 21ST OF JUNE

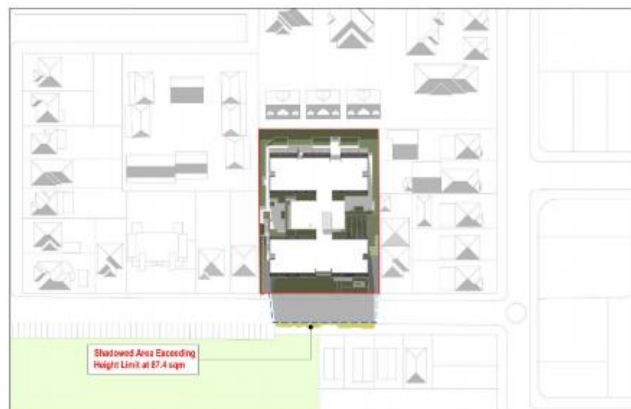


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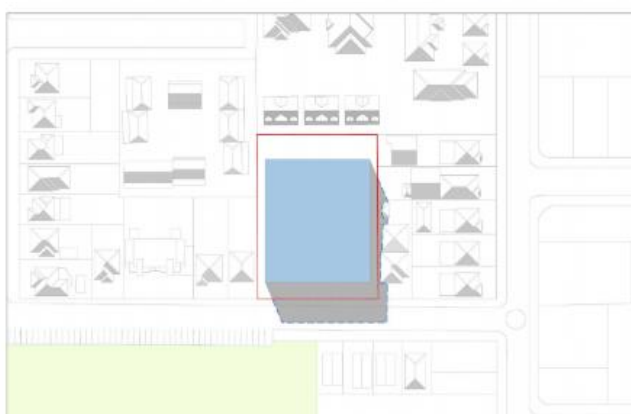




1 COMPLYING ENVELOPE - 11:00 AM - 21ST OF JUNE  
NTS



2 PROPOSED - 11:00 AM - 21ST OF JUNE  
NTS



3 COMPLYING ENVELOPE - 12:00 PM - 21ST OF JUNE  
NTS



4 PROPOSED - 12:00 PM - 21ST OF JUNE  
NTS



**Figure 19** Overshadowing Analysis during the winter solstice

Source: Group GSA

#### 6.4.2 Visual Privacy

The proposed development has carefully considered the visual privacy of the surrounding development through the orientation of the building and design of windows and balconies for the habitable rooms.

The 'H' configuration of the building orientates the rooms so that the windows face north and south to mitigate overlooking onto adjoining properties.

The incorporation of appropriate setbacks and the built form modulation assist in providing the appropriate visual privacy for the surrounding residents.

## 6.5 Views

The proposed development is not considered to have an adverse impact on any local and regional views from surrounding development.

As the proposed development is located on a sloping topography, the maximum building height responds to the topographical changes and the surrounding context. Given the existing height of buildings along the Great Western Highway and orientation of development located on Glebe Street overlooking Ollie Webb Reserve, the proposal will not result in any significant view loss.

Further, while the proposal will be most notable along Glebe Street, it is considered that it provides an appropriate transition in built form, while contributing high quality architectural design to the streetscape. Given the low lying nature of some adjoining development and topographical changes, the proposal will not result in any significant view loss for residential development.

## 6.6 Traffic and Parking

A Traffic and Parking Assessment has been prepared by Varga Traffic Planning and is included at **Appendix H**. The assessment details the potential traffic and transport impact associated with the redevelopment of the site.

### 6.6.1 Traffic Generation

Traffic generation rates from the proposed development have been assessed with regard to the Roads and Maritime Services, *Guide to Traffic Generation Developments – Section 3: Landuse Traffic Generation and the updated traffic generation rates in the RMS Technical Direction (TDT 2013/04a)*. The RMS Guidelines are based on extensive surveys undertaken on a wide range of land uses and nominates the following traffic generation rates which are applicable to the proposed development:

- Housing for Seniors – 0.1-0.2 peak hour vehicle trips per dwelling.

We note that the ancillary facilities within the proposed development are for residents access only and therefore have no bearing on the traffic generation and / or total car parking requirements. Accordingly, the projected future traffic generation is shown in **Table 9** below.

**Table 9 Future projected traffic generation**

Land use	Total traffic generation
Projected Future Traffic Generation Potential:	21.6 vehicle trips
Less Existing Traffic Generation Potential:	-15.6 vehicle trips
Nett Increase in Traffic Generation Potential	6.0 vehicle trips

As seen above, the projected net traffic generation increases by 6 vehicle trips as a consequence of the increase in beds from the existing development. Notwithstanding this, Varga confirms that the projected increase is minimal, consistent with planning controls and will not result in any unacceptable traffic implications in terms of road network capacity to the surrounding street network. Further discussion is provided in **Appendix H**.

### 6.6.2 Parking

The car parking requirements for the proposed development are specified in the Seniors SEPP as follows:

*Clause 48 (d) - Standards that cannot be used to refuse development consent for residential care facilities*

- (i) 1 parking space for each 10 beds in the residential care facility (or 1 parking space for each 15 beds if the facility provides care only for persons with dementia), and
- (ii) 1 parking space for each 2 persons to be employed in connection with the development and on duty at any one time, and
- (iii) 1 parking space suitable for an ambulance.

Application of the above parking rates to the proposed development of 108-beds with a maximum of 29 staff is indicated in the **Table 10** below and further discussion is provided in **Appendix H**.

**Table 10 Parking Spaces in accordance to the Seniors SEPP**

Bed and Staff numbers	Parking spaces
40 standard beds	4.0 spaces
68 dementia beds	4.5 spaces
29 staff	14.0 spaces
1 ambulance	1.0 spaces
<b>Total</b>	<b>23.5 spaces</b>

The proposed development will provide 23 car spaces within the on-site car park and 1 ambulance bay located in the eastern portion of the site. The proposed car parking complies with the provisions of the Seniors SEPP. The design and layout of the car park complies with the relevant Australian Standards.

### 6.6.3 Accessible Parking

One accessibility parking space has been allocated within the 23 parking spaces provided.

### 6.6.4 Service Vehicle Provisions

Service vehicles will include a variety of light commercial vehicles such as vans and small sized trucks. Waste collection will also be undertaken by a private contractor using an 8m long rear-loading garbage truck. A dedicated service area is proposed to be provided at the eastern end of the ground floor level which has been designed to accommodate the swept turning path requirements of an 8.8m long medium rigid truck, as per AS2890.2 requirements.

## 6.7 Noise

An Acoustic Assessment has been prepared by Acoustic Logic and is included at **Appendix K**. The report assesses the potential noise impacts associated with the proposed development. The report has been prepared with regards to the Parramatta DCP, relevant Australian Standards and the NSW EPA - *Noise Policy for Industry (NPI) 2017*.

The assessment illustrates that the major external noise sources to the development will be from vehicle movements along Glebe Street. Accordingly, with regard to the relevant standards, Acoustic Logic have recommended various glazing systems and external wall construction techniques to ensure internal amenity for all residents.

Further, while detailed plant selection has not yet been undertaken, Acoustic Logic confirm that with appropriate acoustic treatments plant noise emissions to the nearest noise sensitive receivers can be controlled.

Accordingly, Acoustic Logic confirm that the proposed development is capable of complying with the relevant acoustic controls as prescribed by the Parramatta DCP, Australian Standards and the NSW EPA - *Noise Policy for Industry NPI 2017*. Further discussion is provided in **Appendix K**.

## 6.8 Heritage

A Heritage Impact Statement has been prepared by John Oultram Heritage & Design and is included at **Appendix Q**. The report assesses the potential heritage impacts associated with the proposed development. The report has been prepared with the regards to the Parramatta DCP and the NSW Heritage Manual "Statements of Heritage Impact" and "Assessing Heritage Significance Guidelines."

John Oultram Heritage & Design has detailed the impact of the development on the following heritage items:

- 41 Great Western Highway – considered remote to the site and the proposed development will not impact its setting or significance;

- 74 and 76 Marsden Street – the site is considered to be set further enough back to have no impact on setting or significance of the heritage item; and
- 78 to 86 Marsden Street – the development will be seen over the roof top of these heritage items, although the development is set some way from the items and is well setback from their street frontages. The Statement confirms the proposal will have a limited and acceptable impact on the setting and significance of these heritage items.

Accordingly, John Outram Heritage and Design consider the proposed development will have a very limited and acceptable impact on the heritage items in the vicinity. Further discussion is provided in **Appendix Q**.

## 6.9 Operational Waste Management

An Operational Waste Management Plan (OWMP) has been prepared by Universal Foodservice Designs and is included at **Appendix N**. The OWMP assesses the proposed waste management services with regard to the Parramatta DCP and the relevant Australian Standards.

Universal Foodservice Design have assessed the estimated volume of garbage and recycling waste generated by the development as shown in **Table 11** and **Table 12** below.

**Table 11 Estimated Volume of Garbage**

Area	General waste litres per day requirement	General waste litres per week requirement
108 residents	540 litres per day	3,780 litres per week.
Main kitchen (estimate)	392 litres per day	2,744 litres per week.
Hair Salon (estimate)	11 litres per day	77 litres per week.
Office areas (Combined estimate)	7 litres per day	77 litres per week.
Multi-Purpose room (estimate)	3 litres per day	21 litres per week.
<b>GENERAL WASTE TOTALS</b>	<b>953 LITRES PER DAY</b>	<b>2,940 LITRES PER WEEK.</b>

**Table 12 Estimated Volume of Recycling**

Area	General waste litres per day requirement	General waste litres per week requirement
108 residents	108 litres per day	756 litres per week.
Main kitchen (estimate)	275 litres per day	1,925 litres per week.
Hair Salon (estimate)	20 litres per day	140 litres per week.
Office areas (Combined Estimate)	12 litres per day	84 litres per week.
Multi-Purpose room (estimate)	5 litres per day	35 litres per week.
<b>CO-MINGLED WASTE TOTALS</b>	<b>420 LITRES PER DAY</b>	<b>2,940 LITRES PER WEEK</b>

A private contractor will be engaged to collect the waste and recycling to an agreed schedule (twice a week) from a dedicated waste collection zone. Maintenance team members and the collection agency will be required to move the required waste bins from the waste management area to the noted waste collection point for pick up before returning the waste bins back to the appropriate waste areas.

The waste collection vehicle will not be reversing into the site's loading dock. Staff will bring waste bins to the loading dock entrance for collection. Further discussion is provided in **Appendix N**.

## 6.10 Construction Waste Management

A Construction Waste Management Plan (CWMP) has been prepared by Universal Foodservice Designs and is included at **Appendix N**. The CWMP assesses the proposed waste management services with regard to the PDCP 2011 and the relevant Australian Standards.

The construction waste vehicle access to the site will be monitored through a detailed site management plan which will be developed by the Managing Contractor to ensure security. It will be the responsibility of the Managing Contractor to coordinate bin movements, waste collection time, and location of bin collection/holding area all in accordance with Australian Standards. Further discussion is provided in **Appendix N**.

## 6.11 Other Impacts of the Development

An assessment of the other impacts of the development have been undertaken by the relevant specialist consultants and are appended to this SEE as set out in **Table 13** below.

**Table 13 Summary of other technical assessments**

Consideration	Consultant	Summary	Reference
Geotechnical	STS Geotechnics	The Geotechnical Report has assessed the subsurface profile and geotechnical conditions within the site. STS Geotechnics tested the subsurface conditions using borehole logs, and encountered asphaltic concrete, astro turf, concrete and fill overlying silty clays and weathered shale. Groundwater was not encountered in any of the borehole logs. STS Geotechnics have provided a number of recommendations in accordance with the relevant codes of practice and standards to be undertaken during the construction and demolition phases.	<b>Appendix P</b>
Contamination	SWE	The Contamination Report has assessed the presence of contaminating activities or indicators of contamination on the site, through soil analysis and assessment. SWE found one fragment of asbestos fibre cement sheeting which is assumed to have washed under the boundary fence from a neighbouring property. Based on these findings SWE believe that the site has low potential to pose an unacceptable risk to human, ecological health or the environment. No further analysis is recommended.	<b>Appendix O</b>
Tree Removal	Tree IQ	A Visual Tree Assessment has been undertaken as discussed in the Arboricultural Impact Assessment. The assessment identified that there are 26 trees within the vicinity of the site, in which 22 will be retained and 4 have been proposed to be removed. These trees have been identified as low landscape significance and have been allocated a Retention Value of Consider for Removal. It has been recommended that replacement of these four trees with healthy, advanced-size specimens could replace the loss of amenity from tree removal within a short timeframe.	<b>Appendix L</b>
Civil and Stormwater	Northrop	<p>The Plans detail the stormwater management strategy in accordance with the PDCP 2011 requirements. Stormwater runoff from the roof area is proposed to be conveyed to an above ground combined onsite detention and reuse tank at the rear of the undercover car park. Runoff from the ground level area is proposed to be conveyed to a pump out tank located within the driveway where captured water will be pumped to the gravity drainage system prior to discharging from the site.</p> <p>The new OSD tank at the rear of the undercover car park. The OSD tank will be a minimum of 158m<sup>3</sup> plus 10m<sup>3</sup> for reuse volume. The proposal also includes water sensitive urban design features as follows:</p> <ul style="list-style-type: none"> <li>• 10KL rainwater reuse tank to harvest runoff from the roof. Water will be reticulated externally for landscaping irrigation;</li> <li>• 4 x oceanguard stormwater pit filter baskets will be provided at ground level;</li> <li>• 7 x tank ocean protect storm-filter to polish runoff from the Level 1 catchment.</li> </ul> <p>It is noted that the proposed development will reduce the impervious area on site by approximately 13% compared to the existing development. Notwithstanding, the new OSD tank is proposed to limit post-developed flows to that of the permissible discharge.</p>	<b>Appendix I</b>
Crime and Public Safety		The built form has been designed to ensure the entry to the site and habitable areas have an outlook to the surrounding public domain to ensure an appropriate level of passive surveillance. The use of a secure and lockable entry system to the building and each floor will ensure residents are safe and only known visitors can enter the facility. Further, non-residential uses are proposed on the ground level including a reception and lobby area and staff facilities. This means that the residential units are not directly accessible for visitors.	



Consideration	Consultant	Summary	Reference
Operational Management Plan	Allity	An Operational Plan of Management (OPoM) has been prepared by Allity. The OPoM detail the management of the proposed facility and staffing requirements. It also includes arrangements for emergency services.	<b>Appendix R</b>
Social and Economic Impacts	-	The proposed development will provide much needed aged care to meet the needs of seniors housing in the LGA. Specifically, the renewed development will help meet the strategic need for providing seniors housing to allow residents to age in place as well as updated communal facilities to enable social interaction amongst residents. Further to this, the proposal will provide additional jobs during both the construction and operational phases supporting the local and regional economy.	-
BCA	MBC	A BCA report has been prepared by MBC to address compliance with the relevant performance requirements of the BCA. The proposed development is capable of complying with the relevant BCA Standards.	<b>Appendix T</b>
Access	Funktion	An Access review has been prepared by Funktion to address the proposed development's compliance against the relevant standards: Disability (Access to Premises- Building) Standards 2010, relevant sections of the Building Code of Australia (BCA) 2019 and Australian Standards on Access and Mobility.  The review confirms that provided the accessible features and recommendations of the report are included in the detailed design phase of the development, the proposal can comply with the relevant accessibility requirements.	<b>Appendix J</b>

## 6.12 Suitability of the site for the development

The proposed development will provide much needed additional aged care to meet the needs of seniors housing in the Parramatta LGA. Specifically:

- The site can appropriately accommodate the proposed development while balancing environmental and design considerations and preserving the amenity of neighbouring properties;
- It will cater for the ageing population and provide capacity for existing residents in the Parramatta LGA to age in place and provide an increased supply and diversity of housing that meets the needs of seniors;
- It will provide a built form that has been architecturally designed commensurate with the surrounding environment;
- The architectural design will deliver a high quality building, which will support a safe and secure aged living environment; and
- The site will provide important infrastructure to service the local and regional area.

## 6.13 Public Interest

The proposed development is considered suitable for the site and is in the public interest as it will:

- Replace the existing RACF with a new RACF, in line with the modern day standards for seniors living, while creating an improved architectural interface with the surrounding public domain;
- Help meet the strategic need for additional housing diversity including the provision of seniors housing in an accessible location;
- It will provide for a modern RACF with considerably improved amenity, functionality and care for residents, in a location close to services and facilities;
- Provide onsite services to enable social interaction amongst residents;
- Increase the provision of community services and connections to the community; and
- Provide significant investment in the aged care sector within the Parramatta LGA that will provide both construction employment and on-going employment once the facility is operational.



## 7.0 Conclusion

The proposed development seeks approval for a three (3) storey residential aged care facility comprising 108 residential aged care beds, services and amenities for use by residents and substantial areas of open space. The proposed development has a number of significant benefits. Specifically, it will support aging in place allowing the members of the local and wider community access to aged care services in an accessible location with the majority of beds being of affordable concessional beds. The proposal also delivers an innovative architectural design that integrates with the local area without having any adverse impacts on adjacent properties or environmental constraints.

This SEE has provided a detailed assessment of the proposal against the relevant matters under Section 4.15(1) of the EP&A Act. The application is recommended for approval as the proposal:

- Is generally consistent with the aims and objectives of the Seniors SEPP and other relevant State Environmental Planning Policies as well as the PLEP 2011 and PDGP 2011;
- Will increase the supply of seniors housing commensurate to forecasted demand within the LGA and improve the quality of seniors housing stock within the LGA;
- Is carefully designed to provide a contextual response to the site setting and minimise bulk and scale impacts to adjoining properties;
- Provides a high quality architectural design that will contribute to a safe and active environment;
- Includes supporting technical studies which confirm that the environmental and social impacts associated with the proposal are generally positive and will not give rise to any adverse impacts; and
- Is suitable for the site and is in the public interest.

In light of the merits of the proposal and in the absence of any significant environmental, social or economic impacts, we have no hesitation in recommending that the application be approved subject to standard conditions of consent.